

PILOT'S OPERATING HANDBOOK CASA Approved Flight Manual

EDGE XT 912 - B STREAK 3 - B MICROLIGHT

Type Certificate No. VA513

Approved By: *[Signature]*

Date: 10 Dec 2004



Delegate of Civil Aviation Safety Authority, Australia



| | | |
|---------------------|-------|--|
| Serial No. Base | XT912 | |
| Serial No. Wing | S3- | |
| Registration No. | | |

THIS HANDBOOK INCLUDES THE MATERIAL REQUIRED TO BE FURNISHED TO THE PILOT BY THE CIVIL AVIATION SAFETY AUTHORITY OF AUSTRALIA AND ADDITIONAL INFORMATION PROVIDED BY THE MANUFACTURER. (Refer to Type Certificate Data Sheet VA513 to determine the correct Pilot's Operating Handbook for Edge XT aircraft)

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PILOT'S HANDBOOK ISSUE DETAILS

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|-----------------------------------|--|
| Pilot Handbook Issued By | |
| Date | |
| For AirBorne WindSports Pty. Ltd. | |

Table 1 Section 0. Pilot's Handbook Issue Details

MANDATORY AIRBORNE DIRECTIVES

AS THE SERVICE HISTORY OF THE AIRFRAME EVOLVES AIRBORNE WILL FROM TIME TO TIME ISSUE DIRECTIVES, WHICH DETAIL ANY CHANGES TO THE MAINTENANCE MANUALS, PILOT'S OPERATING HANDBOOK, OR ANY OTHER DETAILS THAT AIRBORNE DEEMS NECESSARY FOR OWNERS TO BE NOTIFIED OF.

THE WEB ADDRESS FOR AIRBORNE DIRECTIVES IS:

[HTTP://WWW.AIRBORNE.COM.AU/](http://www.airborne.com.au/)

IT IS THE RESPONSIBILITY OF THE OPERATOR TO KEEP UP TO DATE WITH ANY ROTAX DIRECTIVES THROUGH THE ROTAX WEBSITE.

DATA PACKAGE

This Pilot's Operating Handbook constitutes one part of the complete data package that accompanies the aircraft. Following is a list of each of the components, which are required.

- **Pilot's Operator' s Handbook**
- **XT 912 Maintenance Manual**
- **XT 912 Illustrated Parts Catalogue**
- **Streak 3 Wing Maintenance Manual**
- **Streak 3 Wing Illustrated Parts Catalogue**
- **Rotax Owners Manual**
- **Rotax Maintenance (Compact Disk)**
- **Radio Manual – If Installed**
- **BRS Parachute Manual – If Installed**

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Table 2 Section 0. XT 912 Streak 3 Data Package

AMMENDMENT RECORD SHEET

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Manuals will be revised from time to time and re issue of amended pages will be achieved by sending the pages to the current owner registered on AirBorne's data base. Amendments will also be available on the Airborne Website (<http://www.airborne.com.au/>). The amended pages should be printed and the prior page replaced in the manuals folder as soon as possible. The amendment table should at that time be updated with the appropriate details and date.

Table 3 Section 0. Amendment Record Sheet

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| 2-2* | Issue 1.0 | | 4-11* | Issue 1.0 | | 7-4 | Issue 1.0 | |
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| 3-1* | Issue 1.0 | | 4-25* | Issue 1.0 | | | | |

* Indicates APPROVED Pages

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Approved by the Civil Aviation Authority of Australia

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1 GENERAL

This Pilot's Operating Handbook (POH) has various sections to comply with the General Aviation Manufacturers Association (GAMA) handbook specification. The GAMA format has been adopted and used where applicable for this weight shift controlled microlight. Sections 2,3,4,5 and 9 are Civil Aviation Safety Authority approved, and comprise the Approved Flight Manual of the aircraft.

1.1 Introduction

The XT912 Streak 3 has been designed and manufactured in accordance with the Australian Civil Aviation Safety Authority requirements for weight shift controlled aircraft, to the design standard of BCAR Section S, for certification as a primary category aircraft. Australian operational requirements are detailed in Civil Aviation Order 95.32. The operator must be thoroughly familiar with the aircraft and the contents of this manual before initial operation.

As an Australian Company, we are proud of our range of microlight aircraft. Our microlights have been developed to provide the economy and durability required to meet the exacting demands of our Australian conditions.

The success of our microlights is based upon a high standard of product quality, innovative design engineering and exceptional standards of reliability and performance that have been established since 1983.

Regular maintenance is required to keep your microlight in a safe condition. Detailed maintenance requirements are outlined in the Wing and Base maintenance manuals. Please reference these manuals to ensure your microlight is maintained correctly.

In Australia, the Hang Gliding Federation of Australia (HGFA) and the Recreational Aircraft Association (RAA) register and administer microlights.

The AirBorne Team has developed from the long-standing friendship of a group of enthusiasts who share conviction in the intrinsic advantages of weight shift controlled aircraft. The AirBorne Team is confident your new microlight will provide you with many years of enjoyable flying, and we wish you and your family safe and happy flying for the future. The operating procedures outlined in this handbook are the result of Airborne's knowledge and experience gained since 1983.

NOTE

Airborne's data packages will be revised from time to time. It is therefore important that owners promptly notify Airborne of any changes to their contact details. Owners registered on AirBorne's data base will be notified of any changes to data and directed to the AirBorne web site (<http://www.airborne.com.au/>) for the applicable pages. The amended pages should be printed and the replacement pages inserted in the folder as soon as possible. The amendment table should at that time be updated with the appropriate details and date. Revised pages will be sent by mail if requested.

1.1.1 Warning Notice

WARNING

THE OWNER AND OPERATOR MUST UNDERSTAND THAT DUE TO INHERENT RISK INVOLVED IN FLYING A MICROLIGHT/ULTRALIGHT/TRIKE/POWERED HANG GLIDER, NO WARRANTY IS MADE OR IMPLIED, OF ANY KIND, AGAINST ACCIDENTS, BODILY INJURY OR DEATH OTHER THAN THOSE, WHICH CANNOT BY LAW BE EXCLUDED.

THE SAFE OPERATION OF THIS AIRCRAFT RESTS WITH YOU, THE PILOT.

WE BELIEVE THAT IN ORDER TO FLY SAFELY YOU MUST MATURELY PRACTICE AIRMANSHIP.

OPERATIONS OUTSIDE THE RECOMMENDED FLIGHT ENVELOPE SUCH AS AEROBATIC MANOEUVRES OR ERRATIC PILOT TECHNIQUE MAY ULTIMATELY PRODUCE EQUIPMENT FAILURE. YOU ARE REFERRED TO THE OPERATING LIMITATIONS IN SECTION 2 OF THIS MANUAL

THE SETTING UP AND BREAKING DOWN OF A MICROLIGHT/ULTRALIGHT/TRIKE/POWERED HANG GLIDER, TRANSPORTATION AND FLYING WILL HAVE AN EFFECT OVER TIME ON ITS STRUCTURAL INTEGRITY.

THE AIRCRAFT WILL REQUIRE MAINTENANCE AS OUTLINED IN THE APPLICABLE MAINTENANCE MANUALS.

LIKE ANY AIRCRAFT, SAFETY DEPENDS ON A COMBINATION OF CAREFUL MAINTENANCE AND YOUR ABILITY TO FLY INTELLIGENTLY AND CONSERVATIVELY.

WE HOPE THAT YOUR AIRCRAFT WILL PROVIDE YOU WITH MANY HOURS OF SAFE AND ENJOYABLE FLYING.

1.1.2 Definitions

Definitions used in this Pilot Operating Handbook such as **WARNING**, **CAUTION** and **NOTE** are employed in the following context.

WARNING

OPERATING PROCEDURES, TECHNIQUES, ETC. WHICH IF NOT FOLLOWED CORRECTLY, MAY RESULT IN PERSONAL INJURY OR DEATH.

CAUTION

OPERATING PROCEDURES, TECHNIQUES, ETC. WHICH IF NOT STRICTLY OBSERVED, MAY RESULT IN DAMAGE TO THE AIRCRAFT OR ITS INSTALLED EQUIPMENT

NOTE

Operating procedures, techniques, etc. which it is considered essential to highlight.

1.2 General Description

1.1.3 Two View Photos



Figure 1 Section 1. Aircraft Front View



Figure 2 Section 1. Aircraft Side View

1.1.4 General Dimensions

| DIMENSIONS | Australian | European | USA |
|----------------------------------|------------|-----------|------------|
| Wing Span | 9.97 m | 9.97 m | 32.7 ft |
| Wing Area | 13.5 sq m | 13.5 sq m | 146 sq ft |
| Aspect Ratio | 7.4 | | |
| Wing Weight | 50 kg | 50 kg | 110.23 lbs |
| Overall Height (Control Bar Fwd) | 3.65 m | 3.65 m | 11.98 ft |
| Trike Width | 1.91 m | 1.91 m | 6.27 ft |
| Trike Length | 2.745 m | 2.745 m | 9.01 ft |
| Wheel Track | 1.7 m | 1.7 m | 5.58 ft |
| Wheel Base | 1.89 m | 1.89 m | 6.20 ft |
| Trike Height | 2.53 m | 2.53 m | 8.30 ft |
| Cockpit Width | 0.7 m | 0.7 m | 2.30 ft |
| Wing (Packed) Length | 5.6 m | 5.6 m | 18.37 ft |
| Wing Length (Short Packed) | 4.2 m | 4.2 m | 13.78 ft |

Table 1 Section 1. General Dimensions

1.1.5 General Description

1.1.5.1 Base

The XT 912 trike base is a two seat (in line) weight shift controlled aircraft. A Rotax 912 four-stroke engine producing 80 HP powers the base. The layout is typical of this class of two seat trike design, with the pilot passenger "pod" being suspended by a triangular frame, hinged from the mast head about the pitch and roll axes, to provide for weight shift control.

The cockpit has a tall windscreen for improved wind deflection and is cut away at the sides to allow for easier pilot access. The wide rear 6-inch wheels remain a feature of the XT base but the wheel pants/spats have been redesigned and include integrated aerodynamic fins to improve base yaw stability.

1.1.5.2 Wing

The Streak 3 wing is the result of continued refinement of the Streak series wing since 1999. The wing is fairly typical of an established class of swept, tapered, flexible fabric wings. The wing has a relatively high aspect ratio, enclosed cross bars with 80% double surface. It is very stable, and has been designed primarily for cross-country flying.

The Streak sail has several cloth and velcro shear ribs, which combined with an excellent sail "fit", produces a wing that has light handling with impressive "feel" in turbulence. The battens ends are a unique design, which can be adjusted to vary tension for tuning the wing. The batten mechanism allows easy installation and removal of the battens.

An airfoiled aluminium section is used for the down tubes and king post. Clever engineering features fittings that look smart and are positioned to achieve minimum drag.

1.1.5.3 Aircraft

The XT 912 combined with the Streak 3 wing has proven to be an excellent combination. The total fuel capacity is 70 litres and combined with the Rotax 912 delivering a smooth 80 HP allows long cross-country flights.

1.2 Symbols Abbreviations and Terminology

In this handbook:

“**Airfield Pressure Altitude or QNE**” means the altitude of the airfield as indicated on an altimeter with the subscale adjusted to 1013.2 millibars or hectopascals.

“**AUW**” (**All Up Weight**) means the weight of the aircraft including occupants, fuel quantity, engine fluids, and removable and disposable equipment.

“**CG**” means the Centre of Gravity.

“**Empty Weight**” Refer to the note at Section 6.2 for the defined empty weight.

“**fpm**” means feet per minute.

“**HGFA**” means the Hang Gliding Federation of Australia.

“**IAS**” means the airspeed indicated on the cockpit mounted airspeed indicator.

“**kg**” means weight in kilograms.

“**Landing Approach Speed**” means the airspeed that allows control in turbulence, wind gradient or sudden engine failure during landing.

“**Manoeuvring Speed**” means the indicated airspeed above which the pilot may not make full or abrupt control movements.

“**QNH**” – means the pressure setting, that if set on the subscale of a sensitive altimeter, will cause the altimeter to indicate the correct local altitude above mean sea level.

“**RAA**” means the Recreational Aircraft Association.

“**Stall Speed**” means the indicated airspeed at which an uncontrolled downward pitching motion of the aircraft occurs or the forward control bar limit is reached.

“**Take Off Safety Speed**” means the airspeed that allows control in turbulence, wind gradient or sudden engine failure during the climb following take-off.

“**Trim Speed**” means the indicated airspeed at which the aircraft remains in a stabilised condition without pilot input.

“**V_d**” means the aircraft design diving speed.

“**V_h**” means maximum level speed.

“**V_{NE}**” means the indicated airspeed that the aircraft is never to exceed.

1.3 Use of metric / imperial units

This POH uses the metric unit system as the basic system of measurement. Where common usage or available instrumentation refer to the Imperial system both units are quoted. The following conversion factors are presented as a ready reference to the conversion factors that have been used in this manual.

| | | |
|---------------------------------|---|--------------------------|
| 1 Pound (lb) | = | 0.4536 Kilogram (kg) |
| 1 Pound per sq inch (psi) | = | 6.895 Kilopascal (kPa) |
| 1 Inch (in) | = | 25.4 Millimetres (mm) |
| 1 Foot (ft) | = | 0.3048 Metre (m) |
| 1 Statute mile | = | 1.609 Kilometres (km) |
| 1 Nautical mile (NM) | = | 1.852 Kilometres (km) |
| 1 Millibar (mb) | = | 1 Hectopascal (hPa) |
| 1 Millibar (mb) | = | 0.1 Kilopascal (kPa) |
| 1 Imperial gallon | = | 4.546 Litres (l) |
| 1 US gallon | = | 3.785 Litres (l) |
| 1 US quart | = | 0.946 Litre (l) |
| 1 Cubic foot (ft ³) | = | 28.317 Litres (l) |
| 1 Degree Fahrenheit (F) | = | (1.8 X C)+32 |
| 1 Inch Pound (in lb) | = | 0.113 Newton Metres (Nm) |
| 1 Foot Pound (ft lb) | = | 1.356 Newton Metres (Nm) |

Table 2 Section 1. Metric/Imperial Conversion Factors

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2 LIMITATIONS

2.1 General

The limitations section of this POH outlines the various operating limitations, instrument function and placards necessary for the safe operation of this aircraft, engine and standard equipment.

2.2 Airspeed Limitations

| Speed | KIAS | KCAS | Comments |
|-------------------------------------|----------------------|----------------------|---|
| Never Exceed Speed (V_{ne}) | 85 knots (98 mph) | 74 knots (80 mph) | Do not exceed this speed in any operation |
| Maximum Manoeuvring Speed (V_a) | 80 knots (92 mph) | 70 knots (85 mph) | Do not make full or abrupt control movements above this speed |

Table 1 Section 2. Airspeed Limitations

2.3 Airspeed Indicator markings

The standard pressure airspeed indicator on the left side of the dash has a red radial marking indicating 85 KIAS V_{ne} .

2.4 Power Plant Limitations

2.4.1 Engine

Manufacturer: Rotax Bombardier
Model: Rotax 912 UL 2 (4 Stroke)
Gearbox ratio: 2.43: 1

2.4.2 Engine Limitations

| ENGINE LIMITATIONS | Metric | Imperial |
|--|---------------|---------------|
| ENGINE SPEED | | |
| Take Off (Max 5 mins) | 5800 rpm | 5800 rpm |
| Maximum Continuous | 5500 rpm | 5500 rpm |
| PERFORMANCE | | |
| Take -off Performance | 59.6 kW | 80 hp |
| Maximum Continuous Performance | 58 kW | 78 hp |
| OIL PRESSURE | | |
| Max (Allowable for short period at cold start) | 7 bar | 102 psi |
| Minimum (Below 3500 rpm) | 0.8 bar | 12 psi |
| Normal (Above 3500 rpm) | 2.0 - 5.0 bar | 29 - 73 psi |
| OIL TEMPERATURE | | |
| Maximum | 140 deg C | 285 deg F |
| Minimum (Note 1) | 50 deg C | 120 deg F |
| Normal Operating Temperature | 90 -110 deg C | 190-230 deg F |
| CYLINDER HEAD TEMPERATURE | | |
| Maximum | 150 deg C | 300 deg F |
| EXHAUST GAS TEMPERATURE | | |
| Maximum at Max TO power | 880 Deg C | 1620 deg F |
| Maximum at Max Continuous Power | 850 Deg C | 1560 deg F |
| Normal Temperature | 800 Deg C | 1472 deg F |
| AMBIENT START & OPERATING TEMPERATURE | | |
| Maximum | 47 deg C | 116 deg F |
| Minimum | -25 deg C | 13 deg F |
| Maximum at Max Continuous Power | 800 Deg C | 1472 deg F |

Table 2 Section 2. Engine Limitations

NOTE

Minimum Oil Temperature of 50 deg C should be reached before take off. Operate for 2 min at 2000 rpm continue at 2500 rpm until minimum temperature is reached.

2.4.3 Fuel Grades

| FUEL | |
|-----------|--|
| Fuel type | En228 Premium/Regular. Super grade gasoline, lead free, min RON 90 |

Table 3 Section 2. Fuel Specification

NOTE

Due to higher lead content in AVGAS, the wear of the valve seats and deposits in the combustion chamber will increase. Therefore, use AVGAS only if you encounter problems with vapour lock or if the other fuel types are not available.

Use of AvGas requires higher frequency maintenance intervals. Refer to the maintenance manual.

Refer to section 2.12 for fuel capacities and limitations

2.4.4 Lubricating Oil

The 912 UL engine has an external sump, and the entire system is standard to the Rotax 912 engine. The oil specification is given in the Rotax Operators Manual, Section 10.2.3, Lubricants. In general use only synthetic or semi synthetic oil, API classification "SF" or "SG" or later quality oils. Multigrade is recommended. These oil types are detergent types.

Oil Capacity: 3 Litres Max, 2 Litres Min, Consumption 0.06 Litres/Hr Max.

Two oils, which are recommended by the Rotax Service instruction 18, UL 97 for use with both Avgas and Unleaded fuels are:

SHELL, Advance VSX 4, APISG, SAE 15W-50

VALVOLINE, Dura Blend Synthetic, APISJ, SAE 10W-40

2.4.5 Cooling System

WARNING

DO NOT OPEN THE COOLING SYSTEM WHEN THE ENGINE IS HOT. SEVERE SCALDING AND OTHER INJURIES MAY RESULT.

Water-cooling system capacity is 2.5 l. See maintenance manual for further details.

Coolant Specification

A MANDATORY Rotax Directive was issued on the 25th of November 2004, which requires a change in the type of coolant that must be used with the Rotax 912 type engine. By the 23rd Feb 2005, or within 100hrs operating time of the notice, whichever occurs first the new coolant Evans NPG+ waterless coolant is to be used. The reason for the change is "In some instances conventional coolant (mixture ratio of 50% water and 50% antifreeze) can vaporize or boil before the maximum permissible cylinder head temperature is reached." Rotax Service bulletin SB-912-043, pg # 1.

Some Airborne trikes will have a silicate free type high quality and long life antifreeze coolant (which is red), Airborne Part Number 106644, installed in the radiator. This coolant must be changed to the newly recommended coolant by the 25th of Feb 2005, or after 100hrs operating time of the notice.

The directive requires that the new coolant be used, and a sticker be placed on the coolant cap, which prohibits the use of water in the coolant system.

The coolant should be replaced according to the Rotax maintenance manual, current issue. Please refer to the directive, which is available from the Rotax website: SB-912-043, September 04.

WARNING

WATER OR WATER CONTAINING COOLANT MUST NOT BE ADDED IN ANY CASE TO THE COOLING SYSTEM WITH THE NEW EVANS NPG+ COOLANT.

Field service Instructions:

"If EVANS NPG+ coolant is not locally available, temporarily top off the system with propylene glycol antifreeze and be sure not to add water. Within 15 days the temporary coolant should be completely drained and the system refilled with EVANS NPG+ coolant." Rotax SB-912-043, Pg # 5.

2.4.6 Propeller

Manufacturer: Bolly Propellers
Model: BOS3 68 x 58 SL 3B
Type: 3 Blade Composite ground adjustable
Diameter: 68 inch
Pitch: Standard pitch is an angle of 20 +/- 0.5 degrees, at a radius of 24 inches.

The maximum propellor speed of 2387 RPM has been determined by test. The maximum propellor speed occurs when the engine RPM reaches 5800 RPM.

2.4.7 Engine Instrument Markings

The GX 2 instrument has preset alarm limit thresholds. If any of these temperature or pressure limitations are reached the red light on the instrument panel will start to flash. Limitations can be found in section 2.4.2

2.5 Weight limits

| | Metric | Imperial |
|---|-----------|------------|
| Max take off weight | 445 kg | 981 lb |
| Typical empty weight * | 223 kg | 492 lb |
| Maximum empty weight * | 260 kg | 573 lb |
| Permitted range of pilot weights (Front seat) | 55-100 kg | 121-220 lb |
| (Rear seat) | 0-100 kg | 0-220 lb |
| Total maximum crew weight | 200 kg | 440 lb |
| Maximum under seat storage load per bag | 2kg | 4 lb |

Table 4 Section 2. Weight Limits

* Includes 3 liters (2kg) unusable fuel. Empty weight is defined in section 6.2.

2.6 Operational Limits

2.6.1 Centre of Gravity limits

Centre of gravity limits are not critical on the base of a flex wing microlight. Having the trike unit attached to the wing from a single universal bracket, variations of cockpit loading and fuel loading cannot influence the aircraft's balance. The Edge XT is therefore not critical in regards to centre of gravity although the distribution of load in the trike base has a minor affect on the in-flight attitude of the trike base.

| Base Suspension Range | Dimension |
|---|------------------|
| (Measured from the line joining the leading edge nose bolts to the suspension point.) | |
| STREAK III WING | 1245 mm +/- 15mm |

Table 5 Section 2. Centre of Gravity Limits

NOTE

The rear hole on the wing suspension bracket has been blanked off so that it cannot be used. Using the rear hole was found to show non-compliance to the minimum trim speed of 1.3 Vs.

2.6.2 Manoeuvring Limits

All aerobatic manoeuvres including spinning is prohibited.

Aerobatic manoeuvres including whipstalls, stalled spiral descents and negative "G" manoeuvres are not permitted. It must be emphasised that a whipstall, spiral descent or negative G manoeuvre can never be conducted safely. These manoeuvres put the aircraft outside the pilots control and put both the aircraft and its occupants in extreme danger.

Do not pitch nose up or nose down more than 45 degrees from the horizontal. The front support tube of the trike and the pilot's chest limits the fore and aft movement of the control bar respectively.

2.6.3 Bank Angle

Do not exceed 60 degrees of bank angle. In roll there is no stop for the control movement. For the purpose of pre-flight freedom, check by lowering each wing to within 10 cm of the ground (on ground level).

2.6.4 Flight Load Factor Limits

| | |
|--------------------------------------|---------------|
| Max positive manoeuvring load factor | 4.0 G |
| Negative load factors | Prohibited |
| Load factors below 1.0 G | To be avoided |

Table 6 Section 2. Flight Load Factor Limits

2.6.5 Flight Crew Limits

Minimum flight crew is 1 person (Front Seat)

2.6.6 Kinds of Operation Limits

The aircraft is only to be flown under visual flight rules (VFR), and the minimum equipment required to operate under VFR conditions are an Air speed indicator, Altimeter and instruments required by the engine manufacturer.

In Australia, when operated at a public aerodrome or on a cross country flight, a compass and reliable time piece are required. Additional equipment may be required for some overseas operations.

2.6.7 Fuel Limitations

| | | |
|------------------------|----------|--------------|
| Maximum Usable Fuel | 67 litre | 17.7 US Gal |
| Unusable Fuel capacity | 3 litre | .8 US Gal |
| Sump Capacity | 500 ml | 0.132 US Gal |

Table 7 Section 2. Fuel Limitations

CAUTION

SIGHT GAUGE 10 LITRE GRADUATIONS INDICATE TOTAL FUEL, NOT USABLE FUEL. ZERO USABLE FUEL IS INDICATED WHEN THE FUEL IS LEVEL WITH THE BOTTOM OF THE SITE GAUGE

2.6.8 Maximum Passenger Seating Limits

One passenger maximum allowed.

2.6.9 Minimum Pilot Weight

The microlight aircraft must only be flown solo from the front seat. Minimum pilot weight flown solo shall not be below 55 kg. Maximum power at minimum TOW can cause an abrupt climb rate that, if uncorrected, may cause climb angles of greater than the placarded maximum of 45 degrees. Approximately 2/3 of maximum take off power is considered comfortable for a minimum weight takeoff. Take off distance will be extended at reduced power.

2.6.10 Other Limitations

| | | |
|---------------------------------------|----------|-----------|
| Maximum Cross Wind | 12 Knots | 13 mph |
| Maximum Wind Strength | 20 Knots | 23 mph |
| Maximum Ambient Operating Temperature | 47 deg C | 116 deg F |

Table 8 Section2. Other Limitations

No person who is untrained or unqualified in weight shift controlled flight or, who is unfamiliar with the wing and base combination, should ever attempt to pilot the aircraft unless under professional instruction.

The effect of light rain on the aircraft can increase the stall speed. It is extremely important to maintain speeds in excess of the take off and landing safety speeds when the wing is wet. If the aircraft has been left out in the rain or heavy dew it is necessary to wipe the wing down prior to take off. It is also recommended that the aircraft be flown solo first to ensure all excess moisture is removed. A chamois or sponge is recommended to remove the water.

Continued operation in heavy rain is not recommended due to the abrasive effect of raindrops on the propeller. Do not use waterproofing agents on the wing as the consequent beading of water droplets can significantly increase the stall speed.

CAUTION

MOISTURE ON THE WING CAN INCREASE STALL SPEED AND SHOULD BE REMOVED PRIOR TO TAKE OFF.

2.7 Placards

The placards on the aircraft are designed to provide information regarding general aircraft limitations and other details for the safe operation of the aircraft. Listed on the following pages are details of the placards fitted to the aircraft.

2.7.1 Flight Limitations Placard

| | | |
|--|-------------------|----------------|
| 62mm | | |
| ENGINE LIMITATIONS | ENGINE RPM | MAX EGT |
| ROTAX 912 UL | | |
| 5 MINUTES MAX | 5800 rpm | 880 Deg C |
| CONTINUOUS | 5600 rpm | 890 Deg C |
| | MIN | MAX |
| OIL PRESSURE | 0.8 bar | 7 bar |
| OIL TEMPERATURE | 50 Deg C | 140 Deg C |
| CYLINDER HEAD TEMP | | 150 Deg C |
| FUEL TANK CAPACITY | 70 Litres | |
| USEABLE FUEL | 67 Litres | |
| LOADING LIMITATIONS - SEE PILOT'S HANDBOOK SECTION 6.3.3 FOR FUEL LOAD LIMITATIONS. | | |
| EMPTY WEIGHT | | |
| MAX TAKEOFF WEIGHT | 445kg | |
| WEIGHT OF OCCUPANTS | MIN 55kg | MAX 200kg |
| FLY SOLO FROM FRONT SEAT ONLY | | |

PART No.105705

62mm

ENGINE LIMITATIONS

ENGINE RPM

MAX EGT

5 MINUTES MAX

5800 rpm

1620 Deg F

CONTINUOUS

5600 rpm

1560 Deg F

MIN

MAX

OIL PRESSURE

12 psi

102 psi

OIL TEMPERATURE

120 Deg F

285 Deg F

CYLINDER HEAD TEMP

300 Deg F

FUEL TANK CAPACITY

18.5 USGal

USEABLE FUEL

17.7 USGal

LOADING LIMITATIONS - SEE PILOT'S HANDBOOK SECTION 6.3.4 FOR FUEL LOAD LIMITATIONS.

EMPTY WEIGHT

MAX TAKEOFF WEIGHT

981lbs

WEIGHT OF
OCCUPANTS

MIN

121lbs

MAX

440lbs

FLY SOLO FROM FRONT SEAT ONLY

72mm

PART No.105704

| | |
|---|---------|
| 62mm | |
| AIRBORNE WINDSPORTS Pty. Ltd. Newcastle, NSW, Australia www.airborne.com.au | |
| AIRCRAFT TYPE | EDGE XT |
| FLIGHT LIMITATIONS | |
| DO NOT PITCH NOSE DOWN OR NOSE UP MORE THAN 45 DEGREES FROM HORIZONTAL. | |
| DO NOT EXCEED 60 DEGREES OF BANK. | |
| NO NEGATIVE G. | |
| NO AEROBATIC MANOEUVRES. | |
| NO INTENTIONAL SPIN. | |
| NO WHIPSTALLS. | |
| NO STALLED SPIRAL DESCENTS. | |
| APPROVED FOR DAY VISUAL METEOROLOGICAL CONDITIONS ONLY. | |
| WARNINGS | |
| NO SMOKING. | |
| ALWAYS PREFLIGHT AIRCRAFT. | |
| ENSURE MASTER LOCKED IN POSITION BEFORE STARTING ENGINE. | |
| OPERATE IN ACCORDANCE WITH LIMITATIONS AND PROCEDURES DETAILED IN THE PILOT'S OPERATING HANDBOOK. | |

PART No.105706

| | |
|----------|---|
| Location | The flight limitation placards are located on the dash either side of the mast brace. Engine limitations placard option of metric 105705 or imperial 105704 |
| Series | Edge XT |

Table 9 Section 2. Flight Limitations Placards

2.7.2 Dash Placard Locations



Figure 1 Section 2. Flight Limitations, Airspeed, Circuit Breaker, Power Socket and Operators Handbook Placard Locations

2.7.3 Hand Book Placard

18mm
THE PILOT OPERATING HANDBOOK MUST BE CARRIED WITH THE AIRCRAFT. OCCUPANTS MUST BE FAMILIAR WITH INFORMATION NECESSARY FOR SAFE OPERATION.

| | |
|-----------------|---|
| Location | The Hand Book placard is located on the trike dash below the GX2 Instrument |
| Series | Edge XT |

Table 10 Section 2. Hand Book Placard

2.7.4 Wing V_{ne} ASI Placard

| | |
|-----------------|--|
| Location | The Vne Placard is located on the Air Speed Indicator on the left side of dash |
| Series | Edge XT |

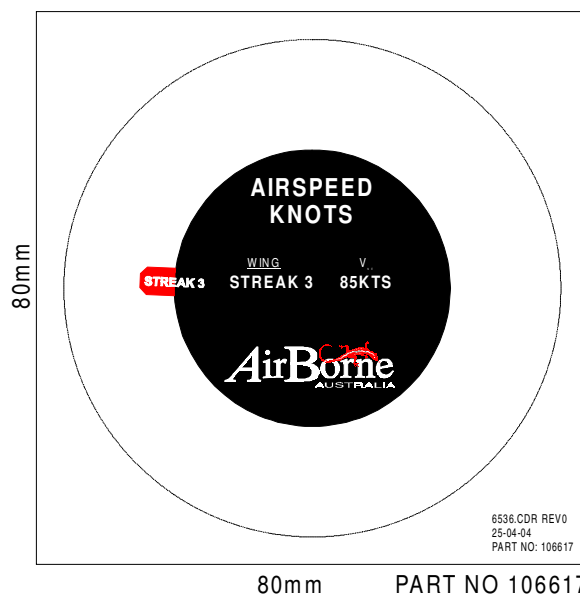


Table 11 Section 2. Wing Vne ASI Placard

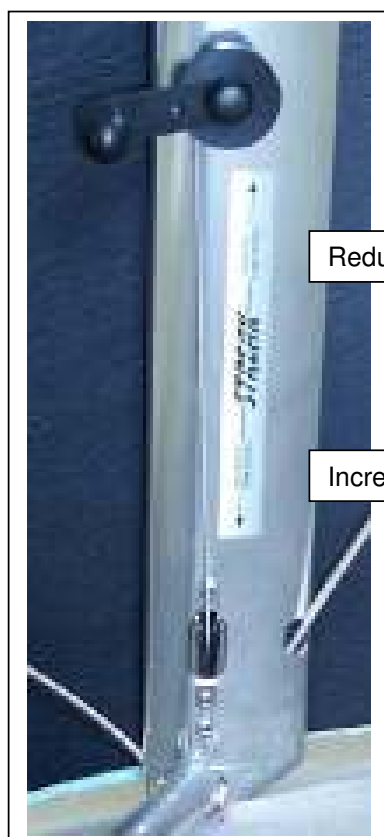
2.7.5 Wing Trimmer Operation Placard



| | |
|-----------------|---|
| Location | The wing trimmer operation placard is located on the right side control frame down tube adjacent to the trimmer knob. The placard is installed so that the "Increase trim speed" arrow faces downward on the down tube. |
| Series | Streak 3 |

Table 12 Section 2. Wing Trimmer Operation Placard

2.7.6 Trimmer Placard Location



Reduce Trim Speed

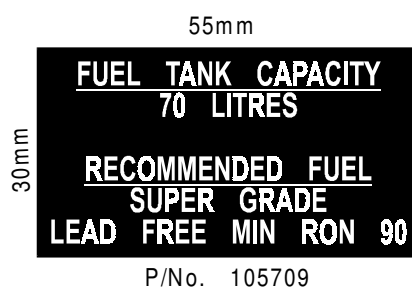
Increase Trim Speed

NOTE

Ensure that the Placard is the correct orientation, as shown by the text boxes beside the photograph.

Figure 2 Section 2. Trimmer Placard Location

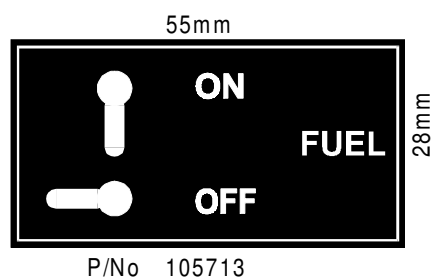
2.7.7 Fuel Capacity Placard



| | |
|-----------------|---|
| Location | The Fuel Capacity Placard is located on the right side shock absorber |
| Series | Edge XT |

Table 13 Section 2. Fuel Capacity Placard

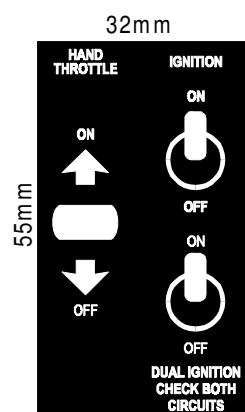
2.7.8 Fuel Tap Placard



| | |
|-----------------|---|
| Location | The fuel tap placard is adjacent to the fuel tap on seat mast block on the left side of the aircraft. |
| Series | Edge XT |

Table 14 Section 2. Fuel Tap Placard

2.7.9 Hand Throttle and Ignition Placard



P/No 105714

| | |
|-----------------|--|
| Location | The hand throttle placard is located on the right side seat frame adjacent to the hand throttle lever. |
| Series | Edge XT |

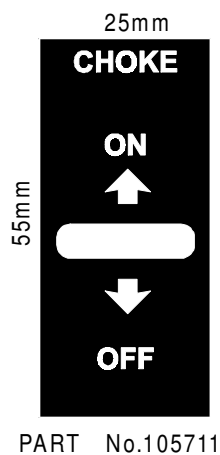
Table 15 Section 2. Hand Throttle and Ignition Placard

2.7.10 Pilots Right Seat frame Placard



Figure 3 Section 2. Hand Throttle and Ignition Placard

2.7.11 Choke Placard



| | |
|-----------------|---|
| Location | The hand choke placard is located on the left side seat frame adjacent to the hand choke lever. |
| Series | Edge XT |

Table 16 Section 2. Choke Placard

2.7.12 Mast Block Placard Locations



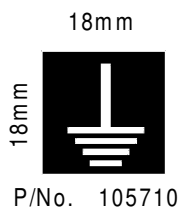
Figure 4 Section 2. Fuel Tap, Shock Absorber Pressure and Earth Placard Locations

2.7.13 Pilots Left Seat frame Placards



Figure 5 Section 2. Choke Placard Location (Under Seat Bag Placard in Background)

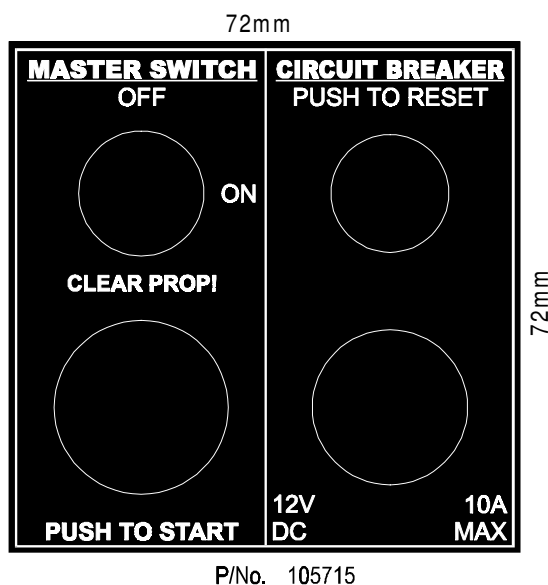
2.7.14 Earth Placard



| | |
|-----------------|---|
| Location | The Engine Earth placard is located on the rear of the seat mast block on the left side |
| Series | Edge XT |

Table 17 Section 2. Earth Placard

2.7.15 Circuit Breaker and Power Socket Placard



| | |
|-----------------|--|
| Location | The Master Switch / Circuit Breaker placard is located on the right side dash. |
| Series | Edge XT |

Table 18 Section 2. Circuit Breaker and Power Socket Placard

2.7.16 Oil Tank Capacity Placard



| | |
|-----------------|--|
| Location | The Oil Tank capacity placard is located on the oil tank on the left side below the engine |
| Series | Edge XT |

Table 19 Section 2. Tank Oil Capacity Placard

2.7.17 No Step Placard



| | |
|-----------------|---|
| Location | The No Step Placards are located on the floor of the cockpit, either side of the base tube. |
| Series | Edge XT |

Table 20 Section 2. No Step Placard

2.7.18 Step Placard



| | |
|-----------------|--|
| Location | The Step Placard is on the trike base tube at the hinge point for the rear foot rest |
| Series | Edge XT |

Table 21 Section 2. Step Placard

2.7.19 Step and No Step Placards



Figure 6 Section 2. Step and No Step Placards

NOTE

There is a symmetrical No Step Placard on the other side of the Pod.

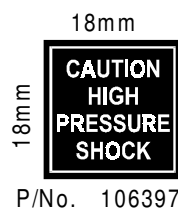
2.7.20 Clear Prop Placard



| | |
|-----------------|---|
| Location | The clear prop placard is located on the right and left side compression struts |
| Series | Edge XT |

Table 22 Section 2. Clear Prop Placard

2.7.21 Shock Absorber Pressure Placard



| | |
|-----------------|--|
| Location | The shock placard is located on the main block near the top of the shock |
| Series | Edge XT |

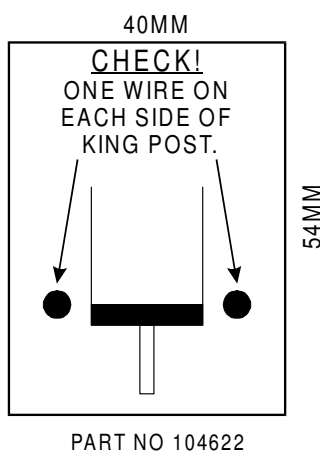
Table 23 Section 2. Shock Absorber Pressure Placard

2.7.22 Right Hand Suspension Strut Placards



Figure 7 Section 2. Clear Prop and Fuel Spec Placards

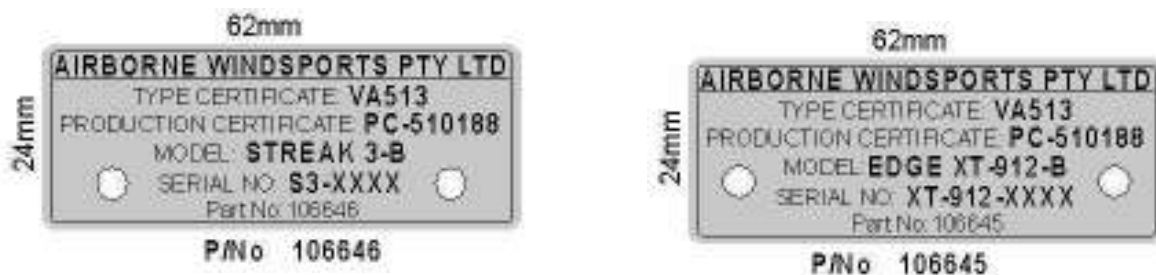
2.7.23 King Post Placard



| | |
|-----------------|---|
| Location | The King Post placard is located on the rear of the keel tube of the wing |
| Series | Streak 3 Wing |

Table 24 Section 2. King Post Placard

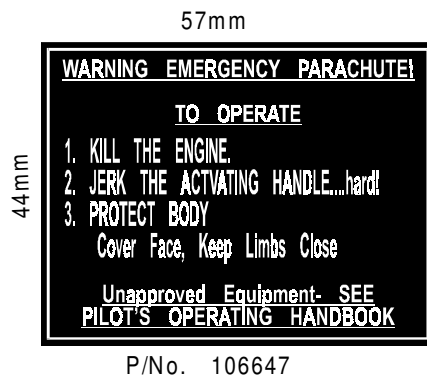
2.7.24 Wing and Base Data Plates



| | |
|-----------------|--|
| Location | The wing data plate is located on the negative block of the universal bracket. The base data plate is located on the seat mast block on the left side of the aircraft. |
| Series | XT912 Streak |

Table 25 Section 2. Data Plates

2.7.25 Emergency Parachute



| | |
|-----------------|---|
| Location | The emergency parachute (when installed) placard is located on the inside left rear of the cockpit when the optional emergency parachute is fitted. |
| Series | XT912 Streak |

Table 26 Section 2. Emergency Parachute

2.7.26 Emergency Parachute Placard Location



Figure 8 Section 2. Parachute Placard Location – (Pilots Left on Pod near to Seat Frame)

2.7.27 Under Seat Placard



| | |
|-----------------|---|
| Location | The Under Seat placard is located under the front seat, one placard per side beside the start of the zip. |
| Series | XT912 Streak |

Table 27 Section 2. Under Seat Placard

This Page left blank intentionally

EMERGENCY PROCEDURES

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|--|----------|
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3 EMERGENCY PROCEDURES

3.1 General

This section of the POH describes the procedures to be adopted in the event of an emergency or abnormal situation occurring in this aircraft.

These procedures are arranged in the sequence considered to be the most desirable in the majority of cases. Steps should be performed in the order listed unless a suitable reason to deviate exists.

This section contains operating procedures for flight and system emergency conditions that are essential for the continued safe operation of the aircraft.

Always maintain correct airspeed and altitudes in the circuit area.

Never fly in uncertain weather conditions and always fly within your proven ability. Be sure only to extend your capabilities under planned training situations.

Carry out safe airmanship whilst flying and be aware of possible emergency landing areas along your flight path. If possible check these areas from the ground as you enter the airfield or flying site. This technique is for safety reasons as engines are susceptible to stopping, no matter how reliably manufactured or maintained.

Keep a good lookout for other aircraft, always be thoughtful and show your intentions. Demonstrate good airmanship always!

It should be remember that the manufacturer cannot foresee all conceivable circumstances. Particular circumstances such as multiple or unanticipated emergencies, adverse weather etc. may require modification to these procedures. A thorough knowledge of the aircraft and its systems is required to analyze the situation correctly and to determine the best course of action.

3.2 Airspeeds for Emergency Operations

| Speed | IAS |
|--------------------------------|-------------------|
| Maximum Manoeuvring Speed (Va) | 80 knots (92 mph) |
| Best Glide | 50 knots (58 mph) |

Table 1 Section 3. Airspeeds for Emergency Operations

3.3 Emergency Procedures Check List

3.3.1 Engine Failure on Climb Out

If your engine fails on climb out, maintain airspeed, reduce angle of attack and land straight ahead if possible. Proceed as follows:

- C** Maintain Control
- A** Maintain Airspeed - take off safety speed
- L** Forced Landing (straight ahead if possible)

3.3.2 Engine Failure at Height

If the engine stops while operating at cruise or full power when the aircraft is well clear of the ground, check:

- C** Fuel Contents
- F** Fuel tap on
- I** Ignition on

If your engine fails in flight, do not attempt to restart the engine unless one of these items is found to be incorrect and is able to be rectified. Relax and maintain control whilst concentrating on correct forced landing techniques.

3.3.3 Full Power Engine Shutdown (In Flight)

If the throttle should jam full open in flight proceed as follows:

- C** Maintain **C**ontrol.
- H** Get **H**eight. With engine at full power adjust height and ground position to improve the outcome of a forced landing.
- A** Increase **A**irspeed to keep the climb angle less than 30 degrees above the horizontal.
- I** Switch off **I**gnition.
- L** Prepare for forced **L**anding

3.3.4 Forced Landings

Proceed as follows:

- C** Maintain **C**ontrol and airspeed - nominated approach speed
- T** **T**hrottle Closed
- I** **I**gnition off
- F** **F**uel tap off
- S** **S**eat belts tight
- H** **H**elmets tight
- L** **L**imbs (arms and hands) inside seat frame
- L** Carry out final approach and **L**anding as closely as possible to normal power off landing procedure.

3.3.5 In Air Engine Fire

For fire occurring whilst in flight, the initial procedure would be to maintain control of the aircraft and evaluate the extent of the fire. This emergency is unlikely to occur but to avoid any further problems, use common sense and land the aircraft safely. Proceed as follows:

- C** Maintain **C**ontrol
- F** **F**uel tap off
- T** Full **T**hrottle (To exhaust engine system fuel as soon as possible and maximise slipstream to clear flames from passengers and airframe.)

When fuel is exhausted then:

- I** **I**gnition off
- L** Forced **L**anding
- B** After landing release seat **B**elt
- P** Release **P**assenger seat belt
- E** **E**vacuate aircraft

3.3.6 On Ground Engine Fire

For fire occurring whilst in motion on the ground proceed as follows:

- C** Maintain **C**ontrol
- S** Use remaining **S**peed to clear people, aircraft and buildings
- T** **T**hrottle closed
- I** **I**gnition Off
- B** After stopping release seat **B**elt
- P** Release **P**assenger seat belt
- F** **F**uel tap off
- E** **E**vacuate aircraft

3.3.7 Propeller Damage

The indication of propeller damage is usually felt by extreme vibration and lack of thrust.

- C** Maintain **C**ontrol
- T** **T**hrottle closed
- F** **F**uel tap off
- I** **I**gnition off
- L** **F**orced **L**anding

WARNING

AT FULL ENGINE REVS THE TIP OF THE PROPELLER IS SPINNING AT SPEEDS IN EXCESS OF 650 KILOMETRES PER HOUR. EVEN SMALL OBJECTS CAN CAUSE SIGNIFICANT DAMAGE TO THE PROPELLER.

This problem may be avoided if precautions are taken prior to take off. Inspect the strip or ground you are to use as your take off area for sticks, rocks or any debris that may be flicked up by the tyres and sucked through the propeller.

Ensure that all items carried by occupants (such as cameras and sunglasses) are secured so they are not able to come loose and pass through the propeller.

3.3.8 Sail Damage

If you encounter damage to the sailcloth during flight, the first procedure is to maintain control of the aircraft. If the sail damage is not impairing the flight characteristics of the aircraft, land at the nearest landing field to inspect the damage.

3.3.9 Emergency Parachute

The emergency ballistic parachute can be fitted as an option.

The parachute-operating handle is fitted with a safety pin. This pin should be removed before each flight and the safety pin must be replaced before the pilot alights from the aircraft. A force of approximately 15 – 20 kg pull on the actuating handle is required to activate the BRS rocket motor

The parachute is only to be used in emergency situations as a last resort and when you are certain that:

- the aircraft has suffered structural damage to the extent that control is not possible; or
- if the aircraft is in an irrecoverable situation where structural damage is likely to occur.

WARNING

IT IS IMPORTANT TO REALISE THAT WHILST THE PARACHUTE CONTROLS THE RATE OF DESCENT, THE PILOT WILL HAVE NO CONTROL OVER THE PLACE THE AIRCRAFT WILL "LAND".

To operate the parachute pull the handle at least twenty centimetres for the parachute rocket projectile to be activated. The parachute will allow the complete aircraft to be lowered to the ground. The aeroplane

will descend with a steep nose down attitude and tilted to the left. Further information can be found in section 7.18.

Proceed as follows:

- T** Throttle closed
- I** Ignition off
- S** Seat belts tight
- P** Check parachute **P**in removed
- D** Deploy parachute
- L** Forced **L**anding

3.3.10 Ignition Circuit Failure

The Rotax engine requires a short circuit on the ignition circuit to stop the engine. If the ignition circuit is broken using full choke to flood the engine should stop the engine.

It is possible to starve the engine by switching the fuel tap off. This method is not as quick as using the chokes.

Do not restart the engine until the fault has been fixed.

3.3.11 Spins and Spiral Descents

Deliberate spinning is prohibited.

A spiral dive may develop after a stall if the bar is maintained at the forward limit and a large roll rate is allowed to develop. If this condition is not corrected it will lead to large and increasing roll attitudes (beyond the 60 degree limit). Increasing attitude, increasing speeds and large control bar feed back forces will occur. Incipient spiral dives can be terminated at any time by rolling wings level. If the spiral dive is allowed to develop to extreme roll attitudes, recovery is expedited by relieving control bar forces before rolling wings level and recovering from high-speed condition.

WARNING
DO NOT ATTEMPT TO SPIN THE AIRCRAFT.

SPIRAL DIVES SHOULD NOT BE ATTEMPTED.

DURING DESCENDING TURNS AIRCRAFT ATTITUDE MUST BE KEPT WITHIN PLACARDED PITCH, ROLL AND AIRSPEED LIMITS.

3.3.12 Unusual Attitudes

Unusual attitudes where the nose is raised or lowered more than 45 degrees from the horizontal are to be avoided. On recognising a situation where the aircraft is approaching these pitch angles proceed as outlined below.

3.3.12.1 Nose High Attitude

To recover from the situation where the nose of the aircraft is pitched up more than 45 degrees from the horizontal proceed as follows:

- H** Hold attitude - Do not attempt to pull control bar in
- P** Reduce **P**ower
- O** As energy dissipates the aircraft will rotate nose down - keep control bar **O**ut.
- P** once the attitude lowers level the wings and increase **P**ower to prevent over pitching.
- R** Recover from dive and **R**esume desired flight path.

3.3.12.2 Nose Down Attitude

To recover from the situation where the nose of the aircraft is pitched down more than 45 degrees from the horizontal proceed as follows:

- O** Raise attitude - push **O**ut.
- P** Apply **P**ower
- R** Recover from dive and **R**esume desired flight path.

3.3.13 **Instrument Failure**

Instrument failure may occur through an electrical fault or through exposure to High Intensity Radio Fields (HIRF).

The aircraft is equipped with an analogue ASI as well as a digital engine management system. The analogue ASI will not be effected by either an electrical fault or HIRF. If there is a problem with the digital system the correct procedure is to fly to the nearest safe landing area and investigate the cause of the malfunction.

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4 NORMAL PROCEDURES

4.1 General

This section of the POH describes procedures for normal operations of this aircraft.

WARNING

NO ATTEMPT SHOULD BE MADE TO FLY THE AIRCRAFT WITHOUT APPROPRIATE WEIGHT SHIFT AIRCRAFT FLIGHT TRAINING WITH AN APPROVED INSTRUCTOR.

4.1.1 Speeds for Normal Operation

| | |
|--|-------------------------|
| Trim Speed | 55-60 knots (63-75 mph) |
| Stall Speed at Maximum Take Off Weight | 35.0 knots (40 mph) |
| Take Off Safety Speed & Nominated Approach Speed at MTOW | 49 knots (56 mph) |
| Maximum Speed in Turbulence (Va) | 80 knots (92 mph) |
| Maximum Level Speed (Vh) | 80 knots (92 mph) |
| Max wind operating conditions (At ground level) | 20 knots (23 mph) |
| Cross winds of up to | 12 knots (14 mph) |

Table 1 Section 4. Speeds for Normal Operation

4.1.2 Normal procedures Check List

This section is provided to supply the pilot with more comprehensive information of the normal procedures required to operate this aircraft and is written assuming the pilot has been trained in the assembly and use of a weight shift controlled microlight.

The ultimate responsibility for determining whether the aircraft is in a safe condition to be flown is with **YOU** the pilot in command. Pre-flight inspections are outlined in the following sections and are your responsibility if you are the pilot in command. Unlike the highway, there is no place to pull over and remedy an unsafe problem once you are airborne.

4.2 Wing Assembly Procedure

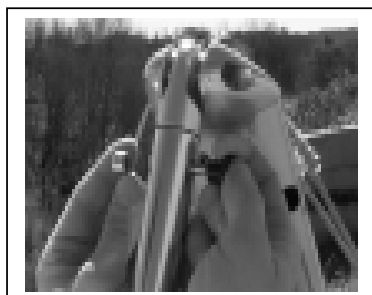
The following sequence of procedures assumes that the wing is packed up. If the wing and base were already assembled this section is not required.

4.2.1 Wing Assembly Procedure

Your instructor should demonstrate the correct assembly and disassembly procedures for your Microlight. This section is intended as a reference only and assumes prior knowledge of assembly. AirBorne trike wings should be assembled standing on the control frame. Assembling the wing on the control frame keeps the sail off the ground and therefore less prone to being soiled or damaged. The suggested assembly procedure is as follows:

UNZIP THE BAG. Lay the wing down with the zip up and the nose facing approximately 120 degrees from the wind direction. Unzip the bag but do not completely remove it from the wing. Undo centre 2 clips.

4.2.2 Assemble Control Frame



Remove control bar and down tube padding. Spread the control bar down tubes out and insert the base bar onto the alloy knuckle. The pip pin is then inserted facing forward. Ensure that the pip pin end cap is secure. It should not be possible to remove the cap without depressing the pip pin button. Check that all the rigging wires are outside the control frame.

Figure 1 Section 4. Assemble Control Frame

Optional:

If training bars are to be fitted to the control frame follow this procedure.

Training bar, showing the correct attachment of the bar on the inside of the control frame. The detail view shows the sequence of components.

1. Bolt, head to the inside of the control frame.
2. Tube
3. Nylon Washer
4. Down Tube Clamp (Both Sides)
5. Wing Nut, turned until both side of the down tube clamp contact. Ensure the training bars are held securely.
6. Safety Pin

Note:

The attachment to the base bar, a pip pin or bolt secures the training bar to the base bar, and also secures the control frame knuckle. A longer Pip Pin is necessary for the larger diameter tube of the Training Bar.

The welded base bar attachment has been made to be slightly loose, for ease of fitment.

The left hand side Training bar is secured in the same way and is also on the inside of the control frame.



Figure 2 Section 4. RHS Training Bar Attachment

4.2.3 Stand The Wing Up

Rotate the control frame to the vertical position so that the wing is resting on the control bar. Do not attempt to connect the nose catch now. Remove the glider bag and unclip all the wing straps

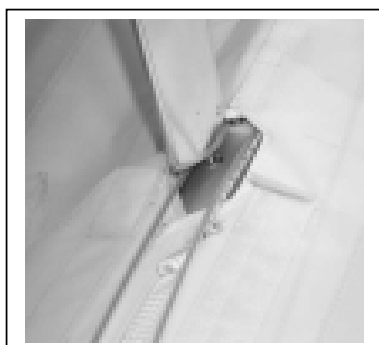


Figure 3 Section 4. Stand the Wing Up

4.2.4 Spread Leading Edges

Carefully spread both leading edges out half way then spread them both out to the approximate flying position. It is essential that the keel and the leading edges are kept in the same plane or damage will result. Each wing should be kept low to the ground whilst moving forward.

4.2.5 Insert King Post



Remove king post base padding and plug the kingpost into the socket on the rear most hole of the keel. Make sure that the cross bar wires are not twisted and are on either side of the king post.

Figure 4 Section 4. Insert King Post

4.2.6 Insert Main Sail Battens

Remove the battens from the bag. Lay out the top surface battens (curved) in order of descending length toward the tip. Place the "red" battens in the left wing (curve forwards), and the "green" battens in the right. Insert the top surface battens except for the last three battens. Start with the battens closest to the keel. The battens are inserted into the pocket with gentle pressure until they meet resistance. When securing the battens lift trailing edge, push fitting in to sail pocket and rotate fitting downward to lock hinge.



Figure 5 Section 4. Insert Main Sail Battens

4.2.7 Tension Cross Bars



Figure 6 Section 4. Tensioning Cross Bar and Shackle Located in Block.

Pull the webbing handle to tension the crossbars. The handle pull back system gives a mechanical advantage of 2:1. Tension until the cross bar wire shackle is located in the quick clip block, behind the safety button.

4.2.8 Install Pull Back Cover

Ensure that the front Velcro tabs are folded back and secured to the top of the cover.

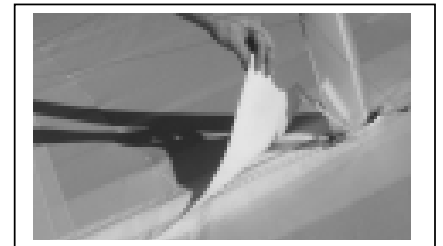


Figure 7 Section 4. Install Pull Back Cover

Attach Nose Catch



The nose catch should now be attached so that the pip pin is inserted through both the nose catch and channel. Ensure the pip pin cap is secure.

Figure 8 Section 4. Attach Nose Catch

4.2.9 Locate Nose Battens

Insert both nose battens tail end first. Locate the front of the batten on the alloy stubs on the front of the keel tube.

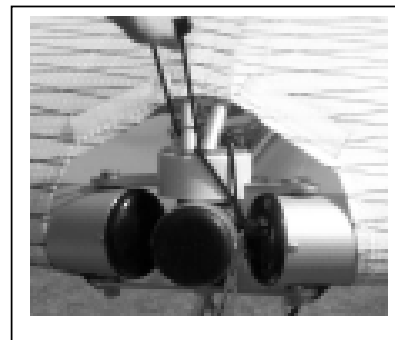
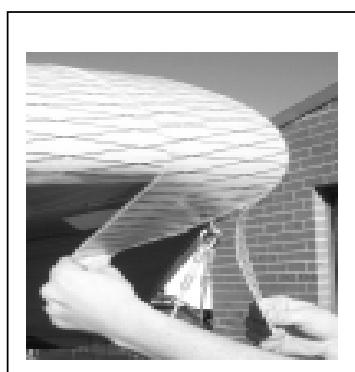


Figure 9 Section 4. Locate Nose Battens

4.2.10 Install Nose Fairing



Attach the nose fairing by applying the top Velcro first then gently tension over the nose plates and attach the Velcro to the undersurface.

Figure 10 Section 4. Install Nose Fairing

4.2.11 Insert Remaining Main Sail Battens

Insert remaining main sail battens at the tips.

4.2.12 Insert Undersurface Battens

The undersurface battens should be inserted as far as possible, without forcing them. The batten should then be pushed with your thumb so that the end is inside the pocket. Use the string to pull the batten back to the rear of the pocket.

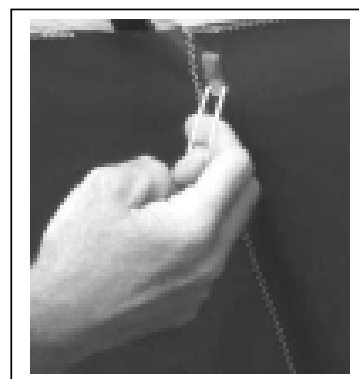


Figure 11 Section 4. Insert Remaining Sail Battens

4.2.13 Load Tip Strut

Reach in through the tip and feed the end of the strut out through the undersurface and locate on red webbing. Pull on the bracket in the center of the strut until the strut over centers. Position hand so that it will not get caught in lever mechanism. (Note photo shows undersurface undone for improved clarity of operation)



Figure 12 Section 4. Load Tip Strut

You are ready for the wing pre-flight inspection. It is imperative that you carry out this inspection **every time you rig and before you fly.**

CAUTION

ONCE THE WING HAS BEEN PRE-FLIGHTED. CHECK THAT ALL INSPECTION ZIPS ARE FULLY CLOSED.

4.3 Wing Pre-flight inspection

The design of the wing is such that junctions not open to view may be reached from zipped inspection panels. Start at the nose and move around the wing making the following checks.

| Wing pre-flight inspection | |
|--|---|
| Nose catch secure and locked | √ |
| Nose Cone Velcro aligned and secure | √ |
| Leading-edge tubing undamaged | √ |
| Cross-bar hinge junction secure | √ |
| Centre undersurface zip secure | √ |
| Sail tip secure and webbing not worn | √ |
| Tip Struts secure and undamaged | √ |
| Battens secure and pockets free from damage | √ |
| Reflex Bridle lines secure | √ |
| Cross bar tensioner secure | √ |
| Velcro Pull Back cover aligned and secure | √ |
| Hang-point / Universal bracket secure | √ |
| Control frame locked | √ |
| Control frame cables secure | √ |
| Top rigging secure | √ |
| All Inspection zips secure | √ |
| Sail condition inspection | √ |
| Sail free from water accumulation | √ |
| General inspection of complete wing | √ |
| Full / free movement of the wing when attached to the trike base – to be completed before flight, see section 4.8.1. | √ |
| Inspect all cables – Inspect for kinks fraying, corrosion – particularly around the NICO press fittings. | √ |

Table 2 Section 4. Wing Pre Flight Inspection

If the wing has been left fully set up for any period, then the following additional checks should be performed:

| Extended set up Wing pre-flight inspection | |
|---|---|
| The symmetry of the wing (Batten profile check). | ✓ |
| All tubes straight, undamaged and without cracks. | ✓ |
| All cables undamaged, no fraying with secure thimbles/swages. | ✓ |
| All nuts and bolts secure and locked appropriately. | ✓ |
| All quick-release fittings secure. | ✓ |
| Universal bracket undamaged, heart-bolt and back-up strap secure. | ✓ |
| Sail tension settings correctly aligned and symmetrical | ✓ |
| Battens undistorted, and in good condition | ✓ |
| All sail seams intact, with no frayed stitching. | ✓ |
| No tears or nicks in the sail. | ✓ |
| Trimmer functional and wires not damaged | ✓ |

Table 3 Section 4. Extended Wing Preflight

4.4 Attaching Wing to Base

WARNING

THE TRIKE MAST IS FITTED WITH A GAS STRUT TO ASSIST LIFTING THE WING. NEVER ALLOW THE MAST TO BE UNLOADED TOO QUICKLY. HANDS OR OTHER FOREIGN OBJECTS WILL BE SEVERELY DAMAGED IF CAUGHT BETWEEN THE MAST AND SEAT / ENGINE BLOCK.

4.4.1 Attach Mast Retaining Strap



The mast has a gas assist strut to assist lifting the wing when the mast is raised. To hold the mast in position for wing attachment the mast retaining strap should be routed around the rear steering bracket and over the mast. The strap can be adjusted to pull the mast down to the correct height.

Figure 13 Section 4. Attach Mast Retaining Strap

4.4.2 Position The Trike And Wing

Position the wing on its control frame, facing into the wind, with the nose on the ground. The mast tube of the trike should be held down using the strap as described above. Check the ignition switches are off. Wheel the trike behind the wing, rolling the front wheel over the control bar. Apply the trike park brake.

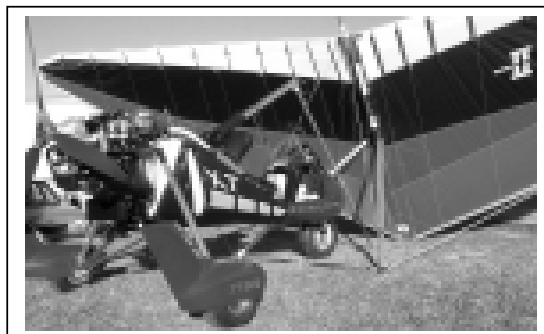


Figure 14 Section 4. Position the Trike and Wing

4.4.3 Attach Mast To Wing U-Bracket



Allow the main tube to rise by loosening the strap until high enough to connect the universal junction to the wing. Insert the bolt with bolt head retaining unit. Tighten wing nut firmly and secure safety pin.. The wing should only be attached using the central hole on the U-Bracket as shown. Remove mast-retaining strap.

Figure 15 Section 4. Attach Wing to Wing U Bracket

4.4.4 Position Heart Bolt Retainer in U Bracket

The standard position for the Heart bolt in the U bracket is in the centre hole, which is the only available point for attachment. The Heart bolt Retainer is positioned in rear open hole to prevent rotation of the bolt.



Figure 16 Section 4. Heart Bolt Retainer Position

4.4.5 Attach Back Up Loop

Connect back up loop so that it passes over the keel and back to the mast. Ensure safety pin is installed.

4.4.6 Remove Keel Extension

Disengage the brake of the trike, lift the nose of the wing to allow the front wheel to be rolled rearward over the control frame so that the base bar is forward of the cockpit. Re engage the brake.

Remove the keel extension by removing safety ring and pulling out the clevis pin. Remove Keel extension. Store with pack up gear.

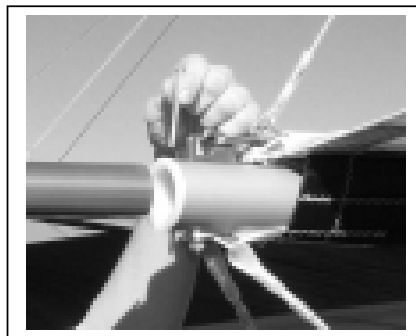


Figure 17 Section 4. Remove Keel Extension

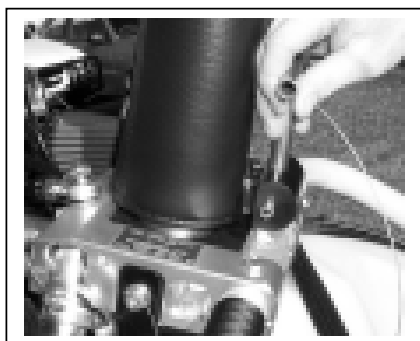
4.4.7 Rotate Wing



Figure 18 Section 4. Rotating Wing

Go to the nose of the wing and with the mast brace tube in one hand and the control bar in the other lift the base bar. Rotate the wing until the rear of the keel rests on the mast. In strong winds maintain a firm grip on the wing.

4.4.8 Insert Mast Lever



Insert the lever so that the flat section is facing toward the mast

4.4.9 Attach Mast Brace

Bring mast brace into position and allow the outer sleeve to slide into position. Install the top pip pin and cap. Install lower pip pin and cap.



Figure 19 Section 4. Insert Mast Lever

4.4.10 Load Mast Lever

Once the lever is located correctly rotate the lever down 180 degrees until it is securely loaded. Fold the seat back up into position.



Figure 20 Section 4. Load Mast Lever

4.4.11 Park The Aircraft



The aircraft should be parked in a crosswind position with the wings base tube secured to the mast brace with the bungee supplied.

NOTE

The wingtip facing the wind should be lowered

Figure 21 Section 4. Park the Aircraft

4.5 Complete Trike Pre Flight inspection

Ensure that the ignition switches are off prior to inspection. Daily inspections as outlined in the Rotax Operator's Manual should be carried out in conjunction with the following inspections.

| Trike base Pre-flight Inspection | |
|---|---|
| No leaks from fuel system and engine. | √ |
| No Leaks from oil system and engine | √ |
| Fuel On/Off valve in the ON position. | √ |
| Fuel filter clean and operational. | √ |
| Fuel drain valve - check for any water in tank sump by draining a small quantity into a container | √ |
| Sufficient fuel for flight. | √ |
| Coolant Level. Between max and min level on coolant bottle. | √ |
| Radiator hoses secure and operational. | √ |
| Oil level OK | √ |
| No splitting, denting or delamination of the propeller. | √ |
| Propeller Hub assembly secure and tie wired. | √ |
| No cracking in tyre treads, or evidence of cracking around the rim. | √ |
| Rear end and Wheel Spats secure | √ |
| No bolts bent, fractured or evidence of corrosion. | √ |
| Electrical system secure and operational. | √ |
| Throttle operation, both foot and hand throttle. Verify free and full movement. | √ |
| Seat belt attachments secure. | √ |
| Steering damper - adjust to desired setting. | √ |
| All engine components secure - air filter, muffler, plug leads. | √ |
| Mast Brace Pip Pins secure | √ |
| Mast Over Centre Latch Loaded and Secure | √ |
| Mechanical Components. Rotate propeller anti clockwise and observe for noise or excessive resistance. | |
| General inspection of complete trike. | √ |
| Wing & Base universal bracket secure. Back up webbing strap secure | √ |

Table 4 Section 4. Complete Trike Pre Flight Inspection

4.6 Fuelling

Fuel flow is from a single fuel tank fitted with a self-venting tube. The fuel system is fitted with a shut off valve located on the rear left hand side of the seat frame. Be sure this valve is in the **ON** position before starting engine.

Never refuel if fuel could be spilled on hot engine components. Use only safety approved fuel containers and never transport fuel in an unsafe manner.

The fuel tank has a water drain mounted at the base and to the rear of the tank. The fuel system has an in-line fuel filter, which is mounted at the front of the tank. This filter can be easily disassembled for cleaning and inspection (See base maintenance manual)

WARNING
ENSURE THE AIRCRAFT IS EARTHED TO
AVOID STATIC DISCHARGE IGNITING FUEL
DURING THE REFUELLING OPERATION

The Edge XT series fuel levels are marked on the right side of the fuel tank. The fuel levels are marked at 10 litres, 20 litres, 30 litres, 40 litres, 50 litres and 60 litres.

4.6.1 Fuel Tank Capacity

The properties of the fuel tank material cause an increase in capacity after the first 2 to 3 tanks of fuel. Initial capacity is 64 litres with the "aged" capacity 70 litres. The fuel level markings have been positioned for the fuel tank capacity at 70 litres.

4.6.2 Fuel Quantity

A sight gauge is provided on the starboard side of the aircraft, visible through the soft side. Its purpose is to provide fuel volume measurement for calculation of aircraft weight during fuelling of the aircraft and to provide the pilot with a visual indication of the quantity of the remaining fuel. The calibration is valid for the aircraft sitting on level ground and indicates total fuel, not usable fuel.

The trike base assumes various flight attitudes according to weight, flight, speed and power effecting the indicated value of fuel quantity in flight. The useable fuel quantity is selected for the worst case condition of aircraft attitude.

Zero useable fuel is indicated by the fuel level reaching the bottom of the sight gauge during level flight.

When the level indicates zero useable fuel, the tank contains 3 litres of unusable fuel.

4.7 Helmet Recommendation

The open cockpit of the Edge XT exposes the occupants to the elements during flight and exposes them to objects outside of the aircraft in an emergency situation.

Helmets and eye protection are recommended for occupants for protection from precipitation, strike by insects and birds. Helmets are also recommended for risk reduction during an emergency landing of the aircraft. The helmets recommended for use in the aircraft are those certified to the EN 966 standard, that is applicable to helmets for air sports. The standard prescribes tests for penetration resistance, shock absorbing properties, field of vision and head mobility.

4.8 Normal Procedures Check List

The following checklists should be used as a reference. More detailed procedures are found in the Amplified Procedures section, which follows.

Prior to flight a thorough pre flight inspection of the aircraft should be carried out. Details of the pre flight inspection are shown earlier in this section.

4.8.1 Before Starting Engine

| | |
|--|-------------------|
| Pre Flight Inspection | Completed |
| Full / free movement of the wing when attached to the trike base | Completed |
| Passenger Briefing | Completed |
| Safety Belts | Secure |
| Helmets | Secure |
| Intercom Connection | Secure |
| Brakes | On / Park |
| Intercom Connection | Secure |
| Parachute (If fitted) | Remove Safety Pin |

Table 5 Section 4. Before Starting Engine Check

4.8.2 Starting Engine

| | |
|------------------------|--|
| Park Brake | On |
| Fuel Cock | Open |
| Hand and Foot Throttle | Off - (Idle Position) |
| Key | On |
| Instrument | On |
| Ignition | On |
| Choke | On |
| Propeller | Clear |
| Depress Start Button | Push - When engine fires Release |
| Oil Pressure | Check - (2 Bar (30psi) within 10 secs) |
| Choke | Off |
| Engine Running | Adjust Idle to 2000 rpm (2 mins) |

Table 6 Section 4. Starting Engine Check

4.8.3 Before Take Off

| | |
|-------------------|--|
| Park Brake | On |
| Choke | Off |
| Warm Up | Adjust Idle to 2500 rpm (Temp to reach 50 deg C) |
| Oil | Check Temperature and Pressure |
| Ignition Check | 4000 rpm Speed drop with only one ignition must not exceed 300 rpm |
| Trimmer | Set Fast (Increase trim speed) |
| Fuel Quantity | Check Sufficient for task |
| Instruments | Check |
| Circuit Breaker | Check |
| Harnesses | Secure |
| Helmets | Secure |
| Throttle Response | Full On (3 seconds) |
| Controls | Pitch and Roll Full and free movement – completed before engine start as well as just prior to take off. |

Table 7 Section 4. Before Take Off Check

4.8.4 Take Off and Initial Climb

| | |
|---------------------|----------------------------------|
| Pitch Control | Neutral |
| Hand Throttle | Off |
| Foot Throttle | Full On (Reduce for minimum TOW) |
| Directional Control | Nose Wheel Steering Straight |
| Rotate at TOSS | 49 KIAS |

Table 8 Section 4. Take Off and Initial Climb Check

4.8.5 Climb

| | |
|---------------|---|
| Foot Throttle | Full On (Reduce for minimum TOW) |
| RPM | 5000 RPM (Reduce to 5000 rpm once climb established. No change if using a reduced power take off) |
| Airspeed | 49-55 KIAS |

Table 9 Section 4. Climb Check

4.8.6 Cruise

| | |
|---------------|-------------------------|
| Hand Throttle | Adjust for Level Flight |
| Airspeed | 55-60 KIAS |

Table 10 Section 4. Cruise Check

4.8.7 Descent

| | |
|---------------|------------|
| Foot Throttle | Reduce |
| Hand Throttle | Off |
| Airspeed | 49-55 KIAS |

Table 11 Section 4. Decent Check

4.8.8 Landing

| | |
|---------------------|------------------------------|
| Hand Throttle | Off |
| Airspeed | 49 KIAS |
| Directional Control | Nose Wheel Steering Straight |
| Braking | Off Then as required |

Table 12 Section 4. Landing Check

4.8.9 After Landing

| | |
|-----------------------|-------------------|
| Parking Brake | On As required |
| Ignition Switch | Off |
| Electrical Switch | Off |
| Radio Equipment | Off |
| Controls | Secure |
| Parachute (If fitted) | Insert Safety Pin |

Table 13 Section 4. After Landing Check

4.9 Amplified Procedures

4.9.1 Before Starting

Safety is everyone's business. Included are only some important safety tips. Keep a good lookout, be thoughtful and always show your intentions prior to starting.

Prior to flight a thorough pre flight inspection of the aircraft should be carried out. Details of the pre flight inspection are shown earlier in this section. Make sure all engine controls are operative and you understand the on/off positions of the throttle and ignition. These controls are readily accessible and you must be able to operate them instinctively without hesitation.

The primary throttle control is foot-operated and complemented by the hand throttle (forward for full power and rearward for power off). The ignition switches are on the right hand side of the seat frame (forward for on and rearward for off)

Never run the engine on the ground with the propeller turning unless you are doing so in a run up area and can observe anyone or anything entering the danger area. It is recommended that the engine not be run for any long periods whilst stationary on the ground. Possible damage to the engine may occur due to overheating of the engine fluid.

Before starting your engine you should read and be familiar with the engine manual.

WARNING
LOCK THE WHEEL BRAKE TO REDUCE ANY POSSIBILITY OF DANGER TO ANY PERSON/S DURING ENGINE STARTING.

Run through the following checklist (pronounced "twimpfish") prior to starting the engine for each and every flight.

- T** Throttle - full and free movement
- Tyres - inflated and serviceable
- W** Wind - check direction and strength
- Wires - secure and airworthy
- M** Mixture - chokes off
- P** Pins - fitted and secured
- F** Fuel - On and sufficient
- I** Instruments - check, set and operational
- S** Switches - ignition check (all switches on)
- C** Controls - pitch and roll - full and free movement
- Chocks - removed (secured in aircraft)
- H** Harness and Helmet in place and secure

Remember that the pilot in command has the ultimate responsibility for the airworthiness of the aircraft in which they fly.

4.9.2 Starting the engine

All controls should be checked with the ignition OFF. Passengers should have seat belts secure and be briefed for the flight.

The engine should be started with the pilot in the front seat. The following procedure should be used:

- Park Brake is locked in the on position
- Fuel cock open

CAUTION
REMEMBER CLEAR PROP!

- Hand and foot throttle off
- Turn key switch and power up instrument.
- Switch both ignitions **ON**.
- Apply full choke unless the engine is hot.
- Check visually that the propeller area is clear and call "**Clear Prop**" out loud
- Depress start button. If the engine refuses to start switch off the ignition before investigation.
- When the engine starts, increase the engine RPM to a little above idle and release the chokes.
- Oil pressure should indicate within 10 seconds.
- Warm up the engine. Minimum Temperature should be reached before take off. Operate for 2 min at 2000 rpm continue at 2500 rpm until minimum temperature of 50 deg C is reached.

WARNING

NEVER LEAVE YOUR AIRCRAFT UNATTENDED WHILE THE ENGINE IS RUNNING.

Keep an aircraft log and enter any unusual engine behaviour. Do not fly unless you have corrected a given problem and recorded the correction in the log.

4.9.3 Taxiing

Taxiing in normal conditions is fairly straight forward.

With the engine idling the brake lever should be depressed which will disengage the park brake. The control frame should be positioned so that it is in the approximate position for normal trim speed. The pilot's feet actuate steering on the ground. Left turn occurs when the right footrest is pushed forward. Right turn occurs when the left footrest is pushed forward.

NOTE

Control sense for turning is opposite to that of a conventional three axis aircraft.

When taxiing in strong wind conditions the following procedures apply:

- **Head Wind** conditions requires the nose of the wing to be lowered just below the trim position
- **Down Wind** conditions requires the nose of the wing to be raised just above the trim position
- **Cross wind** conditions requires the upwind tip to be lowered

4.9.4 Before take off

Before flight a full-throttle check is to be carried out. During this operation the pilot must be seated in the cockpit and prepared to switch off the ignition at very short notice if an emergency should arise.

CAUTION

BEWARE OF LOOSE STONES IN THE RUN UP AREA. LOOSE STONES CAN BE SUCKED UP BY THE PROPELLER AND CAUSE SEVERE PROPELLER DAMAGE IN A VERY SHORT TIME. RUN UPS ARE BEST CONDUCTED ON A CLEAR SEALED SURFACE OR ON GRASS, NEVER ON GRAVEL

The two ignition circuits should be tested with the engine running at 4000 rpm. Ignition one should be switched off and the RPM drop should not exceed 300 rpm. Both ignitions should be in the on position and ignition two should be turned off and the RPM drop should not exceed 300 rpm. Ensure both switches are in the on position after ignition circuit testing.

During take off and landing the recommended trimmer setting is in the fast trim position. It is acceptable to set the trim as far as mid trim position for take off and landing. The trimmer decal on the control frame upright, adjacent to the trimmer knob indicates the trim position.

4.9.5 Take Off

AirBorne trike wings have a neutral static balance allowing a safe take-off that is controllable under all suitable flying conditions.

Take off should be made on full power with only the foot activated throttle used during take off.

The take-off run is the measured ground distance covered until the aircraft reaches a height of 50 feet above the average elevation of the runway used. Refer to Section 5 for details of takeoff performance.

During the take-off run, the wing should be held in the trim position with the wings level. Accelerate smoothly to the take off safety speed. If the aircraft is fully loaded you will require full power.

When the aircraft reaches the takeoff safety speed the control bar should be pushed steadily forward until the trike lifts and rotates quickly on the main wheels. As the aircraft leaves the ground the control bar must be eased back to maintain takeoff safety speed.

Maintain your engine in top condition and assume it's going to stop running at any time. Leave yourself a way out for an unexpected engine failure.

Never fly your aircraft at locations, airspeeds, altitudes, or under any circumstances from which a successful engine off landing cannot be attempted.

4.9.6 Climb

Initial climb out should be made on full power for maximum take off weight. Approximately 2/3 of maximum take off power is considered comfortable for a minimum weight takeoff. Take off distance will be extended at reduced power.

Once climb is established power should be reduced to below maximum continuous power of 5500 rpm. A minimum of takeoff safety speed should be used. At this speed the aircraft would round out nicely into a glide should the engine fail.

Avoid pitching the nose of the wing up more than 45 degrees to the horizon. Very steep climbs are dangerous and can result in a stall followed by a severe pitching of the nose forward. Professional training is required for the correct procedures of unusual attitude recovery.

CAUTION
HIGH-ANGLE CLIMB-OUTS NEAR THE GROUND SHOULD BE AVOIDED.

WARNING
AT LOW ALL UP WEIGHTS, THE TAKE OFF CLIMB OUT AT THE TAKE OFF SAFETY SPEED CAN RESULT IN HORIZONTAL PITCH INCLINATIONS IN EXCESS OF THE PLACARDED 45 DEGREES MAXIMUM. THE PILOT MUST BE AWARE OF THIS AND SHOULD KEEP WITHIN THE PLACARDED LIMITATIONS BY LOWERING THE ATTITUDE OR REDUCING ENGINE POWER.

WARNING
REDUCED POWER TAKE OFFS WILL EXTEND TAKE OFF DISTANCE. IT IS THE PILOTS RESPONSIBILITY TO ENSURE THAT THERE IS SUFFICIENT RUNWAY AVAILABLE TO CLEAR ALL OBSTACLES WHEN CONDUCTING REDUCED POWER TAKE OFFS.

4.9.7 Cruise

When the desired flight altitude is reached the aircraft may be levelled out and throttle reduced to that required to maintain level flight.

The hand-operated throttle on the right side of the seat frame can be used to set engine rpm. Once the hand throttle is adjusted the pressure on the foot pedal may be removed. When the hand throttle is actuated increase power can still be achieved with the use of the foot throttle. The rpm will always return

to the cruise setting when foot pressure is removed. If the hand throttle is set a reduction in RPM is not achievable using the foot throttle. The hand throttle must be in the off position to achieve low RPM.

4.9.8 Stalls

In practice, in level flight it is only possible to induce a nose down stall of the aircraft in level flight at high take off weights. The onset of stall is indicated by a significant increase in control bar loads.

Recovery from a mild stall is very gentle, whether power is on or off. Recovery is quick, with height loss of less than 50 ft with no tendency to break away suddenly. A stall would have to be forced violently, to induce a danger.

When practising stalls make sure you have sufficient altitude. Push the control bar out so that the airspeed is reduced at a maximum of 1 knot per second, and the aircraft will reach a minimum steady flight speed without dropping a wing. The sink rate will increase in this minimum speed mode more than two fold.

If the airspeed is decreased by rapidly raising the nose the wing will stall. Rapid decrease of airspeed in the order of 2-3 knots per second will see an altitude loss of up to 100ft. See section 3.3.12 for recovery procedures.

Never stall with the nose pitched up too high. This is a dangerous manoeuvre and can result in a tail slide followed by a severe tumble. As a guideline, the nose up angle at which the aircraft stalls is about the nose down angle it will recover at.

4.9.9 Descent, Approach and landing

Landing should always be into wind with a long straight approach.

The landing distance specified in section 5 is the measured ground distance covered from an approach at 50 feet above the average elevation of the runway used until the aircraft makes a complete stop.

An approach to the airstrip may be made with or without power, but in either case the airspeed should be maintained above the nominated approach speed.

During take off and landing the recommended trimmer setting is in the fast trim position. It is acceptable to set the trim as far as mid trim position for take off and landing. The decal on the control frame upright, adjacent to the trimmer knob indicates the trim position.

The aircraft should be flown on final approach at or above the nominated safety speed. The additional airspeed allows for wind gradient, and to provide greater controllability in the rough air that may lie close to the ground. Maintaining airspeed on final is very important for engine-off landings, allowing a margin for round out before touchdown.

The trike is designed to land with the rear wheels touching down slightly before the nose wheel. Once firmly on the ground aerodynamic braking may be achieved by pulling in the control bar, then applying the front nose wheel brake.

NOTE

In the case of a heavy landing the maintenance manuals for both the wing and the base should be referenced. It must be noted that after a hard landing, your aircraft must be completely checked.

4.9.10 Cross Wind Landing and Take Off

Pilots with less experience should avoid landing or taking off in conditions with high crosswind components, as skills do not always match the capabilities of the aircraft. Crosswind landings or take off with low wind components up to 8 knots are quite safe and controllable, even to the inexperienced pilot.

WARNING
NEVER STALL THE AIRCRAFT WITH THE NOSE PITCHED UP BEYOND 45 DEGREES. MANOEUVRES BEYOND THIS ARE DANGEROUS AND CAN RESULT IN A TAIL SLIDE FOLLOWED BY A SEVERE TUMBLE.

REFER TO SECTION 3.3.12 OF THIS FLIGHT MANUAL FOR DETAILS OF THE PROCEDURES FOR RECOVERY FROM UNUSUAL ATTITUDES

The nominated approach speed of 49 KIAS should be increased to 55 KIAS when landing in cross wind conditions of 10 knots or more.

After touchdown in cross wind conditions the relative airflow over the wing will become increasingly span wise (From tip to tip) as the aircraft slows down. The upwind wing tip should be lowered slightly (the amount depends on the wind strength), and the undercarriage wheels will retain firm contact with the ground.

Take off procedure is unchanged for the nominated crosswind limit. The upward wing may need to be lowered at the start of the take off procedure in higher cross winds.

4.9.11 Baulked Landing

During a situation where a baulked landing (go around) is required, normal take off power and procedures should be used.

4.9.12 Stopping the Engine

To stop the engine after a period of running, the ignitions should be switched off at idle. Switching off at high RPM floods the engine and makes restarting difficult. If the engine has been running under full power allow the engine to cool at idle, before switching off

4.10 After Landing / Securing

After landing and when in the parking area apply parking brake and lock. Switch the ignition, Electrical switch and radio equipment off. The aircraft should be parked in a crosswind position with the base tube secured to the mast brace with the bungee supplied. The emergency parachute safety pin should be inserted before leaving the aircraft.

4.10.1 De Rigging Procedure

Careful attention to the recommended rigging and de-rigging sequences will protect the aircraft from the risk of unnecessary damage.

The de-rigging procedure is a direct reversal of the rigging procedure. A summary of the procedure follows:

4.10.1.1 Remove Wing from Base

See section 4 (Attaching Wing to Base) and use reverse procedure

- Apply park brake.
- Remove pin pins from the front support compression tube. Slide outer mast brace up and insert pin pin through lower hole (This will secure the inner and outer tubes for the lowering phase).
- Unload and remove over centre latch.
- Lower the wing until the control bar is on the ground.
- Secure mast with strap to base tube.
- Unbolt the trike from the U Bracket; remove safety loop and wheel out the trike unit.
- Reinstall keel extension tube.

4.11 Wing Break Down Procedure

This section assumes that the wing has been removed from the base. The wing should have the keel extension fitted with the clevis pin and ring installed. This section is intended as a reference only and assumes prior knowledge of the break down procedure. Further reference for cover positioning can be found in the wing Illustrated Parts Catalogue (IPC).

4.11.1 Detension Tip Struts

Fold the tip struts pushing the hinge joint towards the center of the wing. Once the sail end of the strut is inside the undersurface fold the strut forward and towards the tip of the wing and locate on top of leading edge.

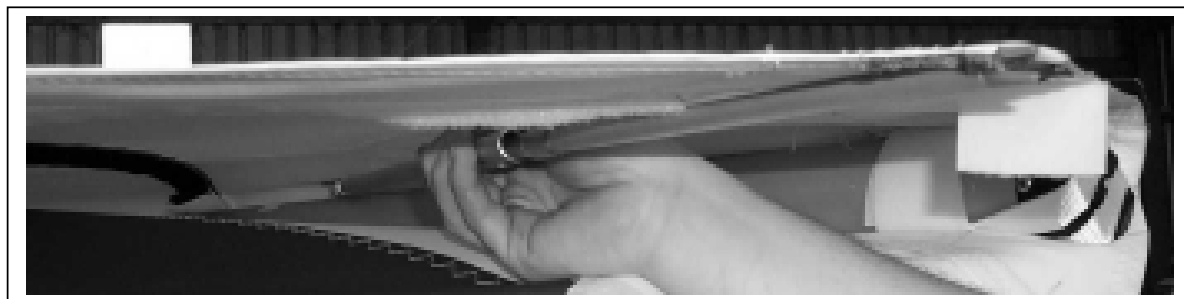
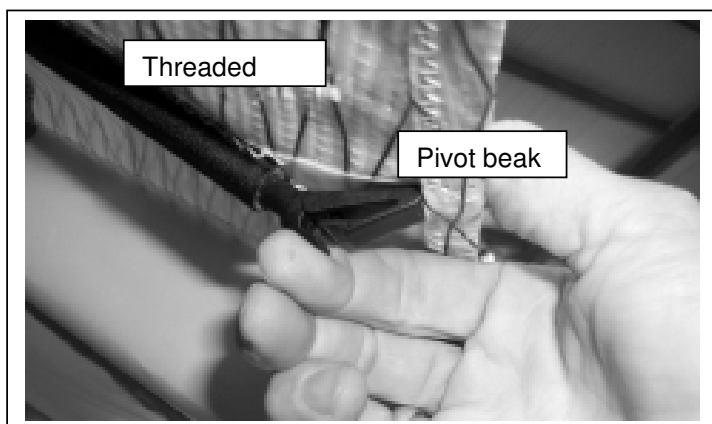


Figure 22 Section 4. Detension Tip Strut

4.11.2 Remove Tip Battens



Remove outermost three tip battens. Unclip 'pivot beak' from 'threaded end'. Rotate *pivot beak* and remove from sail as shown. To adjust batten load tension, release *pivot beak* from sail and rotate batten clip. See maintenance manual for adjustment details.

Figure 23 Section 4. Remove Tip Battens

4.11.3 Remove Undersurface Battens

Insert finger through string loop and pull batten forward. Once the batten is forward pull string down to remove from oval pocket. Slide batten rearward until all the way out.

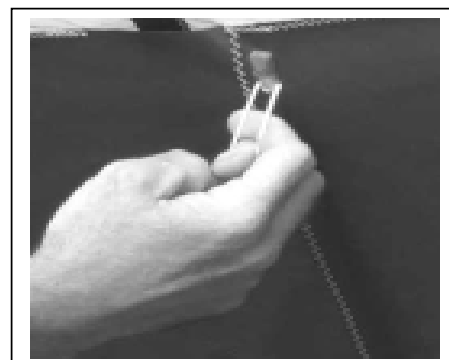


Figure 24 Section 4. Remove Undersurface Battens

4.11.4 Fit Tip Bags



Roll sail at the tips inwards and parallel to the leading edge. Slide the tip bag over the end of the leading edge. The barrel lock unit can be used to tension the bag bungee.

Figure 25 Section 4. Fit Tip Bags

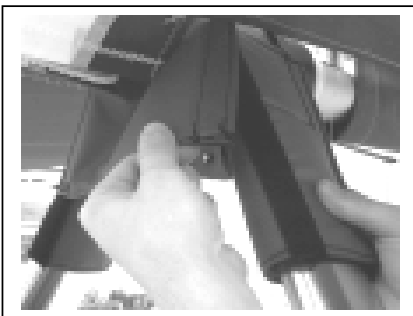
4.11.5 Fit U-Bracket Cover

Unzip undersurface approximately 1 meter to allow better access to fit the U Bracket cover. Pass the cover up over the keel with the webbing strap facing to the rear of the wing. Velcro around each down tube. (Photo shown is a rear view)



Figure 26 Section 4. Fit U Bracket Cover

4.11.6 Velcro U-Bracket Face Cover



Position the face cover so that it is facing forward. The cover is designed to protect the wing bag when packed. (Photo shown is a front view)

Figure 27 Section 4. Velcro U-Bracket Face Cover

4.11.7 Trimmer Cover

Position the trimmer handle so that the handle is at 90 degrees to the down tube with the knob facing forward. Fit the trimmer cover.

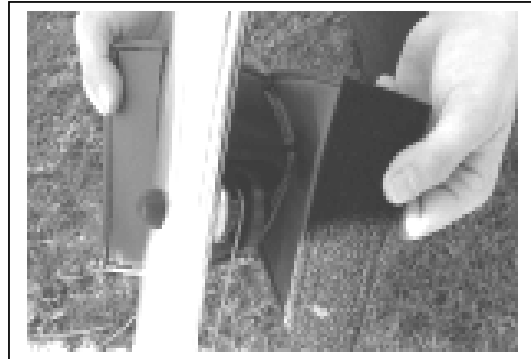
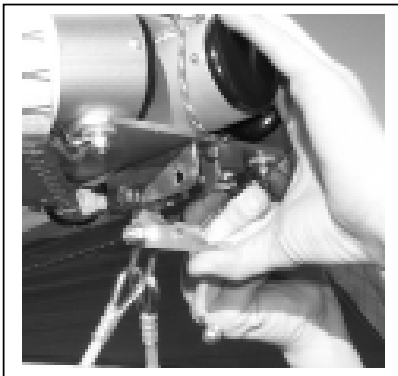


Figure 28 Section 4. Trimmer Cover

4.11.8 Disconnect Nose Catch and Remove Nose Cone



Remove the nose cone. Disconnect nose catch by removing the pip pin. The pip pin button should be depressed with the thumb while simultaneously pulling the pip pin out. Re-insert pip pin in nose channel.

Figure 29 Section 4. Disconnect Nose Catch and Remove Nose Cone

4.11.9 Remove Sail Cowling

Undo the securing Velcro tabs at the front of the cowl. Remove the sail cowling from the top of sail.

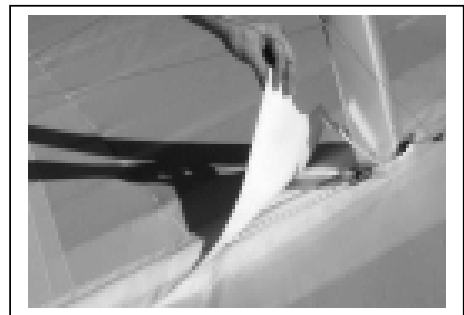
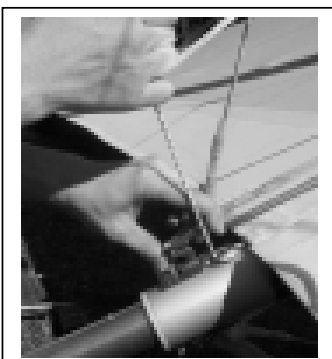


Figure 30 Section 4. Remove Sail Cowling

4.11.10 De-Tension Pull Back Cable



Depress quick clip with left thumb. Use right hand to pull on the webbing handle. Remove the shackle from the quick clip block and allow webbing handle to move forward.

Figure 31 Section 4. De-tension Pull Back Cable

4.11.11 Remove Main Sail Battens

Pull the leading edges together approximately 1/2 meter. Remove the remaining main sail battens. Insert battens in the batten bag.

NOTE

The straight battens are inserted in separate pockets.

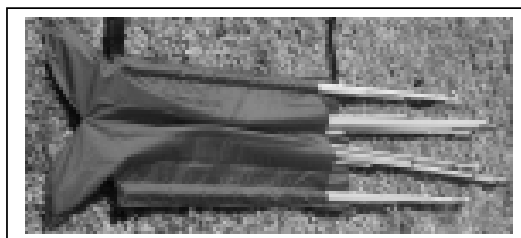


Figure 32 Section 4. Remove Main Sail Battens

4.11.12 Remove King Post



Remove the king post by lifting upward. Fit the Quick Clip King Post Base cover around the quick clip. Insert the king post in to pouch.

Figure 33 Section 4. Remove King Post

4.11.13 Fold Leading Edges

Fold both wings in symmetrically, bringing both leading edges back at the same time or in small steps side to side.

Roll the sail inwards parallel to the leading edge. Attach a strap around one wing. Repeat for the other side. Fit the keel end pouch.



Figure 34 Section 4. Fold Leading Edges

4.11.14 Attach Straps

Once the leading edges are together apply slight pressure downwards on the keel to raise the leading edges above the down tubes and attach strap around both wings and keel. Attach remaining straps so that they are evenly spaced

4.11.15 Fit Wing Bag

Position the wing bag to the nose of the wing. Stretch bag down the wing to enclose the tips.

4.11.16 Roll Wing

Hold the wing and down tube and roll the wing onto its back with the control frame to the side.

4.11.17 Disconnect Base Bar



Depress pip pin button and remove pip pin from base bar. Fold down tubes together with base bar folded out. Re insert the pip pin.

Figure 35 Section 4. Disconnect Base Bar

4.11.18 Fit Padding

Fit the base bar and down tube covers. Velcro the base bar cover in whilst holding the base bar in position.



Figure 36 Section 4. Fit Padding

4.11.19 Fold Control Frame

Undo centre two wing straps. Route flying wires between the down tube and lower the control frame into the wing whilst holding tension on wires. Avoid kinking the flying wires.

4.11.20 Position Battens



Stow the battens with the curve down at the rear of the glider bag. Reattach wing straps so that the control frame and battens are within the leading edge pockets. Zip up the wing bag.

Figure 37 Section 4. Position Battens

4.12 Transportation and Storage

The wing must always be transported inside its bag, and the bag zip should face downwards to prevent the entry of rainwater. During transportation, or when stored on slings, the wing must be supported at its centre and at two points not more than one metre from each end. The padding supplied with the wing must be used to prevent chaffing during transport.

Supports should be softly padded, and any support systems used for transport, such as roof racks, must use attachment straps that are sufficiently secure to eliminate the possibility of damage from vibration and movement.

Avoid damage to your wing by using well-padded racks. As the wing is quite heavy a strong set of racks are required. Flat straps should be used for tie downs to avoid damage to leading edge Mylar.

When transporting the trike base the use of trike and prop covers to protect your aircraft from road grime (and idle fingers) is recommended. Tie the propeller to the trike to stop it from rotating at speed.

Check that the back of the wing is well clear of the front mast with the trike on the trailer. Remember that you have an overhanging load when manoeuvring in tight places.

Store the wing in a dry room off the ground; air the wing out regularly to avoid mildew, and never store wet.

See your Rotax Manual for precautions to be observed if you intend to store the aircraft without use for extended periods.

4.13 Noise Characteristics

The Edge XT 912 Streak has been certificated to UK Air Navigation (Environmental Standards) Order 2002, schedule 3 for two seat microlight aeroplanes. Noise levels were recorded at 78.2 dB(A).

Airservices Australia have found compliance to CAO 101.55 with resultant noise levels of 62.7 dB(A).

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PERFORMANCE

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5 PERFORMANCE

5.1 General

The performance data in the following section has been computed from actual flight tests with the aircraft and power plant in good condition and using average piloting techniques. It should be noted that piloting techniques, climatic conditions and aircraft condition will cause significant variation to these performance figures.

5.2 Take Off and Landing

Take Off

| Performance at MTOW | Australian | European | USA |
|-----------------------------------|------------|----------|--------|
| Take off Distance to 15 m (50 ft) | 247 m | 247 m | 810 ft |

Table 1 Section 5. Take Off

Takeoff distances are specified for:

Sea Level with:

Max Take Off Power

A level dry runway

With short grass

Still wind

And temperature of 15 deg C.

The following factors will increase takeoff distance:

Reduced Power Take Off

Higher drag runway surfaces such as wet or long grass

Tail wind

Uphill takeoff

OAT above 15 deg C

Runway altitude above sea level

The pilot is required to take into account the effect of the above when determining takeoff distance.

Landing

| Performance At MTOW | Australian | European | USA |
|------------------------------------|------------|----------|---------|
| Landing Distance from 15 m (50 ft) | 325 m | 325 m | 1066 ft |

Table 2 Section 5. Landing

Landing distances are specified for:

Sea Level with

A level dry runway

With short grass

Still wind

And temperature of 15 deg C.

The following factors will increase landing distance:

Lower drag runway surfaces such as tarmac

Tail wind

Down hill landing

OAT above 15 deg C

Runway altitude above sea level.

The pilot is required to take into account the effect of the above when determining landing distance.

Crosswind components of up to 12 knots at maximum AOW are within aircraft operating limitations.

Always exercise judgement when selecting locations for take-off and landing. Leave adequate margin for appropriate control action in the event of sudden engine failure or turbulence being encountered.

CAUTION
TAKE OFF AND LANDING DISTANCES MUST
BE INCREASED BY 20% FOR EACH 1000
FEET OF ALTITUDE ABOVE SEA LEVEL.

5.3 Climb

| Performance at MTOW | Australian | European | USA |
|---------------------|------------|-----------|------------|
| Climb rate (49 kts) | 825 ft/min | 4.2 m/sec | 825 ft/min |
| Best Climb Speed | 49 kts | 91 km/hr | 57 mph |

Table 3 Section 5. Climb

Climb data is for ISA conditions (Sea Level at 15 deg C)

Sea Level Gradient of Climb is 17.8%

5.4 Airspeed Calibration

All Air Speeds in this POH are expressed as Knots Indicated Air Speeds (KIAS) unless otherwise noted. Below is a table showing the relationship between Indicated Air Speed and Calibrated Airspeed. The table assumes zero instrument error.

| Indicated Air Speed (KIAS) | Calibrated Air Speed (KCAS) |
|----------------------------|-----------------------------|
| 35 | 35 |
| 40 | 39 |
| 45 | 43 |
| 50 | 47 |
| 60 | 55 |
| 70 | 63 |
| 80 | 70 |
| 85 | 74 |

Table 4 Section 5. Airspeed Calibration

5.5 Stall Speeds

| Performance | Australian | European | USA |
|--------------------------|------------|----------|--------|
| Stall Speed @ MTOW | 35 kt | 65 km/h | 40 mph |
| Stall Speed @ 320 kg TOW | 31 kt | 57 km/h | 36 mph |

Table 5 Section 5. Stall Speeds

5.6 Glide

Glide figures have been determined with the engine off at maximum take off weight with the trimmer set in the fast configuration

| Performance - 54 kts at MTOW | Australian | European | USA |
|--------------------------------|------------|----------|----------|
| Descent Rate | 770 ft/m | 3.9 m/s | 770 ft/m |
| Descent Gradient | 15.3% | 15.3% | 15.3% |
| Glide Distance from 1000ft AGL | 4.6 km | 4.6 km | 2.5 Nm |

Table 6 Section 5. Glide

Glide data is for ISA conditions (Sea Level at 15 deg C)

5.7 Cruise

| Performance at MTOW | Australian | European | USA |
|---------------------------------------|------------|-----------|-------------|
| Cruise Speed | 65 kts | 120 km/hr | 75 mph |
| Typical Fuel Burn @ Cruise (See Note) | 12 lt/hr | 12 lt/hr | 3.17 gal/hr |
| Range @ Cruise | 702 km | 702 km | 436 miles |

Table 7 Section 5. Cruise

NOTE

Fuel consumption figures are included as a guide only. The consumption figures should not be used for planning purposes. Changes in aircraft configuration, load, altitude, wind strength and direction as well as climatic conditions will cause significant variation in fuel consumption.

WEIGHT BALANCE AND EQUIPMENT

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6 WEIGHT BALANCE AND EQUIPMENT

6.1 General

This aircraft must only be flown solo from the front seat. All aircraft operations may be carried out whilst solo, as when the aircraft is flown dual.

The fuel capacity must always be considered when measuring the AUW of the aircraft. Remember that fuel is measured at 0.7 kg per litre and fuel quantity will alter the aircraft's performance during take off and landing. A fuel volume calculator can be found in section 6.3.2.

6.2 Aircraft Weight

The table in section 6.2.1 shows the weight of the aircraft as weighed during final Quality Assurance at the factory. The following page shows, if applicable, the options that were included when the aircraft was weighed. The empty weight shown below can also be found on the limitations placard on the aircraft dash panel. A second section is included which allows the weight to be revised if any additional equipment is installed or if the empty weight of the aircraft changes for any reason. If the empty weight has changed then revised placards are available from Airborne. The weight calculators allow for additional weight to be accounted for.

6.2.1 Aircraft Weighing Information

| Aircraft Type: AirBorne WindSports EDGE XT 912 STREAK | | | | |
|---|---------------------------|-------|------|--------------------------------|
| | Serial No | Issue | Date | Empty ¹ Weight (kg) |
| Trike Base | XT912- | 1 | | kg |
| Unusable Fuel | 3 Litres | 1 | | 2 kg |
| Streak III Wing | ST3- | 1 | | 50 kg |
| Training Bars | 1.4 kg (If applicable) | 1 | | |
| Issue 1 Empty Weight Edge XT 912 Streak* | | | | kg |

| | | | | |
|---|---------------------------|---|--|-------|
| Trike Base | XT912- | 2 | | kg |
| Unusable Fuel | 3 Litres | 2 | | 2 kg |
| Streak III Wing | ST3- | 2 | | 50 kg |
| Training Bars | 1.4 kg (If applicable) | 2 | | |
| Issue 2 Empty Weight Edge XT 912 Streak* | | | | kg |

Table 1 Section 6. Aircraft Weight

*Empty Weight for the Edge XT 912 Streak comprises of:

- Standard equipment as per section 6.2.2
- Optional equipment as per section 6.2.3
- Full coolant, full engine oil and unusable fuel

The keel extension and pack up gear are not included in the empty weight

6.2.2 Standard Equipment

| STANDARD EQUIPMENT | Fitted |
|---------------------------------------|-----------------------|
| Engine Type | ROTAX 912 UL 2 |
| Engine Serial Number | |
| Carburettor Heating | Std |
| Gear Box Type | 2.43 : 1 |
| Propeller Type | BOLLY BOS 3 - 66 INCH |
| Airspeed Indicator (Knots) | Std |
| GX2 Instrument | Std |
| Tool Kit & Mast Strap Hold Down Strap | Std |

Table 2 Section 6. Aircraft Weight Standard Equipment

6.2.3 Optional Equipment

| OPTIONAL EQUIPMENT | FITTED (Yes/No) |
|-------------------------|--------------------|
| BRS Emergency Parachute | |
| Radio | |
| Intercom | |
| Training Bars | |
| | |

Table 3 Section 6. Aircraft Weight Optional Equipment

6.3 Typical Aircraft Weights

| | | |
|--|--------|--------|
| Empty Weight + 172 kg crew + 1 hr Fuel (18 litres /13 kg) | 406 kg | 895 lb |
| Empty Weight + 86 kg pilot + full fuel (70 l / 49kg) | 356 kg | 785 lb |

Table 4 Section 6. Typical Aircraft Weights

The wing should be lifted when assembled by routing a webbing strap around the king post top. The trike base should be lifted by the suspension point on the top of the mast.

6.3.2 Weight Calculator

The weight calculator has been designed to assist in the calculation of maximum allowable fuel so that MTOW does not exceed 445kg. The calculator uses the typical empty weight of the microlight as stated above. Adjustments must be made if the microlight is not the "typical weight" i.e. if additional options are included.

Find the intersection of the pilots weight versus the passenger weight (plus additional weight), this gives the maximum amount of fuel to be used without exceeding the MTOW.

Airborne XT 912 Streak Fuel Volume Calculator (Metric)

| | | Front Occupant kg | | | | | | | | | |
|---|---|-------------------|----|----|----|----|----|----|----|-----|--|
| | | 60 | 65 | 70 | 75 | 80 | 85 | 90 | 95 | 100 | |
| Rear Occupant +20 kg Additional equipment | Occupant+10 kg Additional Equipment | 0 | | | | | | | | | |
| | | 5 | | | | | | | | | |
| | | 10 | | | | | | | | | |
| | | 15 | | | | | | | | | |
| | | 20 | | | | | | | | | |
| | Rear Occupant kg | 0 | | | | | | | | | |
| | | 5 | | | | | | | | | |
| | | 10 | | | | | | | | | |
| | | 15 | | | | | | | | | |
| | | 20 | | | | | | | | | |
| 25 | | | | | | | | | | | |
| 30 | | | | | | | | | | | |
| 35 | | | | | | | | | | | |
| 40 | | | | | | | | | | | |
| 45 | | | | | | | | | | | |
| 50 | | | | | | | | | | | |
| 55 | | | | | | | | | | | |
| 60 | | | | | | | | | | | |
| 65 | | | | | | | | | | | |
| 70 | | | | | | | | | | | |
| 75 | | | | | | | | | | | |
| 80 | | | | | | | | | | | |
| 85 | | | | | | | | | | | |
| 90 | | | | | | | | | | | |
| 95 | | | | | | | | | | | |
| 100 | | | | | | | | | | | |

Tank filling volume calculated in liters

UNIT CONVERSION

| Volume | | Weight | |
|------------|--------|--------|----|
| Gallons US | Litres | lb | kg |
| 0.2642 | 1 | 2.205 | 1 |
| 1.3 | 5 | 11.0 | 5 |
| 2.6 | 10 | 22.0 | 10 |
| 4.0 | 15 | 33.1 | 15 |
| 5.3 | 20 | 44.1 | 20 |
| 6.6 | 25 | 55.1 | 25 |
| 7.9 | 30 | 66.1 | 30 |
| 9.2 | 35 | 77.2 | 35 |
| 10.6 | 40 | 88.2 | 40 |
| 11.9 | 45 | 99.2 | 45 |
| 13.2 | 50 | 110.2 | 50 |
| 14.5 | 55 | 121.3 | 55 |
| 15.9 | 60 | 132.3 | 60 |
| 17.2 | 65 | 143.3 | 65 |

Fuel capacity 70L subject to:

MTOW of 445kg

Payload of 222kg

Empty weight of 223kg

Additional equipment:

eg BRS of 11kg

Fuel consumption at full power

18 L/Hr

| | |
|-------|-----|
| 154.3 | 70 |
| 165.3 | 75 |
| 176.4 | 80 |
| 187.4 | 85 |
| 198.4 | 90 |
| 209.4 | 95 |
| 220.5 | 100 |
| 231.5 | 105 |
| 242.5 | 110 |
| 253.5 | 115 |
| 264.6 | 120 |

11L= 36 minutes fuel at full power

Example: Pilot 75kg, 10kg additional equipment, Passenger 100kg = 53L fuel

Table 5 Section 6. Metric Fuel Calculator

6.3.4 Imperial Fuel Calculator

| Airborne XT 912 Streak Fuel Volume Calculator Imperial (US) | | | | | | | | | | | | | |
|---|-------------------------|---------------|--|------|-----------|------|------|------|------|------|------|------|------|
| lb Additional equipment near Occupant +20 | lb Additional Equipment | Rear Occupant | Front Occupant | | | | | | | | | | |
| | | | 120 | 130 | 140 | 150 | 160 | 170 | 180 | 190 | 200 | 210 | 220 |
| 60 | 80 | 100 | | | | | | | | | | | |
| 70 | 90 | 110 | | | | | | | | | | | |
| 80 | 100 | 120 | | | | | | | | | | | |
| 90 | 110 | 130 | | | | | | | | | | | |
| 100 | 120 | 140 | Tank filling volume calculated in US gallons | | | | | | | | | | |
| 110 | 130 | 150 | | | | | | | | | | | |
| 120 | 140 | 160 | | | | | | | | | | | |
| 130 | 150 | 170 | | | | | | | | | | 18.6 | 16.9 |
| 140 | 160 | 180 | | | | | | | | 18.6 | 16.9 | 15.2 | |
| 150 | 170 | 190 | | | FULL TANK | | | | 18.6 | 16.9 | 15.2 | 13.6 | |
| 160 | 180 | 200 | | | | | 18.6 | 16.9 | 15.2 | 13.6 | 11.9 | | |
| 170 | 190 | 210 | | | | | 18.6 | 16.9 | 15.2 | 13.6 | 11.9 | 10.3 | |
| 180 | 200 | 220 | | | | 18.6 | 16.9 | 15.2 | 13.6 | 11.9 | 10.3 | 8.6 | |
| 190 | 210 | | | | 18.6 | 16.9 | 15.2 | 13.6 | 11.9 | 10.3 | 8.6 | 6.9 | |
| 200 | 220 | | | 18.6 | 16.9 | 15.2 | 13.6 | 11.9 | 10.3 | 8.6 | 6.9 | 5.3 | |
| 210 | | | 18.6 | 16.9 | 15.2 | 13.6 | 11.9 | 10.3 | 8.6 | 6.9 | 5.3 | 3.6 | |
| 220 | | | 18.6 | 16.9 | 15.2 | 13.6 | 11.9 | 10.3 | 8.6 | 6.9 | 5.3 | 3.6 | 1.9 |

UNIT CONVERSION

Volume

| Gallons US | Litres |
|------------|--------|
| 0.2642 | 1 |
| 1.00 | 3.8 |
| 3.77 | 14.3 |
| 5.43 | 20.6 |
| 7.10 | 26.9 |
| 8.76 | 33.2 |
| 10.43 | 39.5 |
| 12.09 | 45.8 |
| 13.76 | 52.1 |
| 15.42 | 58.4 |
| 17.08 | 64.7 |
| 18.75 | 71.0 |

Weight

| lb | kg |
|-------|-----|
| 2.205 | 1 |
| 1 | 0 |
| 10 | 5 |
| 20 | 9 |
| 40 | 18 |
| 70 | 32 |
| 120 | 54 |
| 160 | 73 |
| 200 | 91 |
| 240 | 109 |
| 280 | 127 |
| 320 | 145 |
| 360 | 163 |
| 400 | 181 |
| 440 | 200 |
| 480 | 218 |
| 503 | 228 |
| 520 | 236 |
| 560 | 254 |
| 600 | 272 |
| 640 | 290 |
| 992 | 450 |

Fuel capacity 18.5 gal(US) subject to:
MTOW of 981 lb
Payload of 489.4 lb
Empty weight 491.6 lb

Additional equipment:
eg BRS of 24.25 lb
Fuel consumption at full power
4.75 gal / hr
3.6 gal = 45 minutes fuel at full power

Example: Pilot 200 lb, 20 lb additional equipment, Passenger 190 lb = 13.6 gallons of fuel maximum

Table 6 Section 6. Imperial Fuel Calculator

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7 AIRCRAFT & SYSTEMS DESCRIPTION

7.1 General

This section provides descriptions of the aircraft and its systems as well as methods of operation where appropriate.

Information on the aircraft flight controls is detailed in this section, but it is mandatory that you receive professional training prior to any solo flight. It is illegal to operate this aircraft in Australia without a licence issued by the HGFA or RAA.

7.2 Airframe

Wing

The Streak III is a high performance wire braced weight shift controlled micro light wing. The airframe is constructed from 6061-T6 Multi sleeved aluminium tubing.

The keel and cross tubes are totally enclosed in the double surface. With the keel running parallel to the relative airflow, a decrease in drag is achieved. More importantly it enables the trike to be suspended from the aerodynamic centre of the wing giving greater roll and pitch authority. The Streak III is light and predictable in both the roll and pitch axis.

The multi sleeved leading edge construction is 63.5, 60.0 and 57.0 mm tube. This large diameter construction designed along with the sail luff curve, acts to preload the leading edge and maximise trailing edge tension throughout the speed range.

The sail is constructed using latest technology from the sail making industry. The leading edge is constructed using a PX 20 Mylar cloth. The main-sail cloth is 6 oz Dacron with a Mylar insert in leading edge pocket. A trailing edge band provides minimum stretch when loaded that not only gives long sail life, but also improves the top speed by controlling twist.

The Streak III wing has been load tested in excess of 2450 kg. Excellent engineering contributes to the relatively lightweight of 50 kg.

Base

Attached to the wing by way of a universal joint is the trike base. The universal joint allows the free movement of the trike base in pitch and roll by which control is effected. The trike base includes the characteristic tricycle undercarriage, power plant and cockpit.

The engine is mounted to the engine platform at the base of the engine. A compression rod is used at the top of the engine and attached to the seat / mast block. A long-range fuel tank is mounted beneath the engine platform.

The pilot cockpit is designed to allow for various size pilots. The standard instruments used on the XT are a Skydat GX2 and additional analogue airspeed indicator in knots. The cockpit has soft sides attached to the pod and encloses the trike base tube and most of the fuel tank.

The maximum tyre pressure is 30 psi (205 kPa) and optimum pressure for general operations is 15 psi (103 kPa).

7.3 Flight Controls

Flight controls are as follows:

- Control bar move right = Left turn
- Control bar push out = Pitch up
- Push right toe = Throttle open
- Hand throttle forward = Throttle open
- Tighten trim cable = Slow trim

7.4 Ground / Flight Control

Ground Controls are as follows:

- Push left pedal = Taxi steering right
- Push Left Toe = Brakes on
- Ignition switch forward = Switch on
- Choke forward = Choke on
- Fuel Tap Aligned with tap body = Fuel on

7.5 Trimmer operation

The Streak III trimmer system allows in flight trim adjustment by rotating the trimmer wheel on the right downtube. Rotation of the trimmer wheel clockwise raises the reflex bridles causing extra reflex in the root section of the wing. This causes a reduction in trim speed of 10-15 knots. The swage on the trimmer wire is used as a pointer on the trimmer placard.

During take off and landing the recommended trimmer setting is in the fast trim position. It is acceptable to set the trim as far as mid trim position for take off and landing. The decal on the control frame upright, adjacent to the trimmer knob indicates the trim position.

There is a slight increase in roll pressures as the trimmer is used to decrease trim speed.

The aircraft is designed to be stable at trim under all loads with a small increase in trim airspeed as the AUW is increased.

7.6 Instrument panel



The instrument panel consists of an analogue airspeed indicator (knots) on the left side with the digital GX2 instrument mounted centrally in the dash. A power circuit breaker, power switch, 12V DC outlet and starter button can be found on the right side of the dash

7.7 Undercarriage System

The microlight uses a tricycle undercarriage with a braking system via a nose wheel drum brake unit. Oil dampened front shock absorbers are used.

The rear suspension is a swinging wish bone design in conjunction with a 45mm compression strut which houses an oil pneumatic shock absorber.

7.8 Seat Adjustment

The front seat back on the Edge XT trike has a simple for and aft adjuster on the base tube. To adjust the seat backrest forward the following procedure should be adopted:

- 1) Remove pin and bolt from channel.
- 2) Rotate seat back forward and replace bolt in channel one hole further forward.
- 3) Replace safety pin through bolt.

To move backrest rearward the bolt should be moved one hole back.

7.9 Occupant restraint Harness

Both front and rear seats are fitted with a 3-point restraint harness system. The shoulder inertia reel system is fitted to the mast and requires attachment on the male section of the lap belt during lap belt fastening (see photo)

When flying the trike solo it is important to fasten the rear seat belt to prevent contact with hot engine components in flight



7.10 Engine

The power unit is a Rotax 912 UL 80hp 4 stroke engine designed and built in Austria. The Rotax engine is fitted with a gearbox, which delivers smooth thrust via a reduction drive. This power unit is complemented with a ground adjustable propeller giving the ultimate in performance and reliability. The engine is fitted with Bing carburettors with an external dry filter.

7.11 Carburettor heating

The system is designed to minimize the risk of carburetor icing. A heater block is attached directly to the carburetor body with radiator coolant passing through the block. The heat from the carburetor body is also transferred to the carburetor spindle and butterfly. No significant heating of the intake air takes place so there is negligible loss of engine power. All flight performance data has been obtained with the heaters fitted and operational.

The carburetor heating system will work automatically when on. An occasional check that the heater bodies are getting warm is advisable after engine running.

The system has not been tested under all possible conditions that may prevail, therefore its effectiveness cannot be guaranteed in all circumstances. Aircraft equipped with this device should never be flown in circumstances where a successful 'no power' landing cannot be made in the event of engine failure.

7.12 Propeller

The aircraft is equipped with a 3-blade ground adjustable pitch composite propeller. The hub is an anodised alloy, which is ground adjustable. The propeller is 66 inches in diameter. The propeller pitch is set for the certification of the aircraft at 20 Deg. The pitch setting and checking procedure is given in the Base Maintenance manual. Settings outside this specification have an unknown effect on aircraft performance, and are not approved.

7.13 Brake System

A front wheel drum brake system is used on the aircraft. Depressing the brake lever on the left hand side of the front footrest actuates the brake.

7.14 Electrical System

An electrical schematic for the aircraft is shown in the diagram on the following page.

The Electrical circuits comprise:

- an instrumentation circuit. The 12 V DC supply is protected by a 20 amp fuse at the battery and a 10 amp circuit breaker mounted on the dash. The master switch on the dash, when in the off position, disables the DC power socket, flight instrument and the electric start push button;
- an engine management circuit; and
- an ignition circuit.

It should be noted that the ignition circuit is a fail-safe system whereby the engine will run in the event of the ignition circuit becoming disconnected. Switching the coil to ground stops the engine.

When stopping the engine both switches on the side of the seat should be switched off. The master switch on the dash should then be turned to the off position to remove supply to the accessories.

If necessary the motor can be stopped using the chokes as detailed in section 3.3.10 of this handbook

Refer to the Rotax manual for more details for the engine electrical system.

The diagram illustrates the electrical system for a vehicle, featuring a battery, alternator, starter motor, and various control switches. Key components and their connections include:

- Battery:** Connected to the main power distribution bus.
- Alternator:** Labeled "ALTERNATOR" and connected to the battery via a "BATTERY LINK".
- Starter Motor:** Labeled "STARTER MOTOR" and connected to the battery through a "STARTER RELAY".
- Ignition Switch:** Controls the "IGNITION SYSTEM" and "BATT+IN".
- Lighting and Accessories:** Includes "HEADLIGHTS", "TAIL LIGHTS", "STOP LIGHTS", and "HORN".
- Wiring Loops:** Various loops are shown, such as "BLACK WIRING LOOP", "RED WIRING LOOP", and "GREEN WIRING LOOP".
- Test Points:** Numerous test points are indicated throughout the circuit for diagnostic purposes.

At the bottom left, there is a reference note: "REFER TO 912 INSTALLATION MANUAL SECTION 17, PG. 40. INSTALL 912 ELECTRIC SYSTEM."

Figure 1 Section 7. Electrical Schematic

7.15 Pitot Static System and Instruments

The pitot static system supplies ram air pressure to the air speed indicator from the nose of the cockpit. The static pick up is at the rear of the instrument under the dash

7.16 GX2 Instrument Function

The GX 2 instrument has preset alarm limit thresholds. If any of the temperature or pressure limitations are reached the red light will start to flash.

Standard instrumentation includes the AMPtronics GX2 Digital Flight instrument (see description below) and an analogue airspeed indicator in knots.

WARNING

IT IS PROHIBITED TO FLY THIS AIRCRAFT WITH THE AMPTRONIC SKYDAT GX2 ALARM THRESHOLDS SET OUTSIDE THE ENGINE MANUFACTURER'S LIMITS.

7.16.1 Description of Features

The GX2 is a combined avionic instrument with programmable functions. The system consists of two parts: the display module, which is mounted in the dash, and the capture module, which is located on the engine tie rod. The photo shows the layout of the display when set up for the 912 Rotax engine.

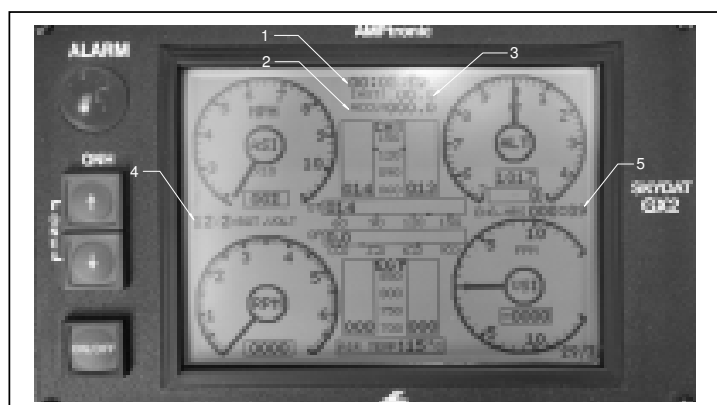


Table 1 Section 7. GX2 Display

GX2 Features

| | |
|---|--|
| ASI mph , knots or kilometers | ALT (Altimeter) in feet or meters |
| RPM (engine revolutions per minute) | VSI (vertical speed indicator) in m/s or ft/min |
| Flight duration (1), in hours; minutes; seconds | Battery Voltage (4) |
| Engine Hours (5), in hours, minutes | 2X CHT (cylinder head temperature) Celsius or Fahrenheit |
| Oil Temperature | 2X EGT (exhaust gas temperature) Celsius or Fahrenheit |
| Oil Pressure, bars or PSI | Air Temperature |

Optional

Fuel flow data available if fitted with optional fuel flow sender.

| | |
|--|------------------------------------|
| Accumulative fuel (2) in liters or gallons | Fuel Flow (3) in liters or gallons |
|--|------------------------------------|

Table 2 Section 7. GX2 Features

7.16.2 System Turn On

Turn key clockwise to the on position. Press the “**ON/OFF**” push button of the panel. The alarm lamp will flash briefly. If the lamp does not flash it should be rectified prior to flight as the flashing lamp indicates an over temperature situation.

The display will light up prompting you to reset the flight duration to zero by pressing “**QNH+**”. If the fuel flow option is installed, to reset the accumulated fuel consumed press “**QNH-**”. After a few seconds the different engine measurement indications will be displayed. The green LED on the capture module should be continually flashing.

The preset engine limits can be checked against the limits outlined in section 2.4.2 by noting the position of the larger square bars on the LCD display for the particular gauge.

Altimeter Adjustment

You can adjust the barometric pressure as follows:

QNE - Depress both buttons ↑↓ on the left of the instrument simultaneously to set at 1013mb

QNH - Depress ↑ to increase altitude pressure. Depress ↓ to decrease altitude pressure.

The pressure display is right under "ALT" in the center of the altimeter. The readout below pressure display is a digital altimeter reading.

The preset limits can be checked against the limits outlined in section 2.4.2 by noting the position of the larger square bars on the LCD display for the particular gauge.

Changing units system

At any time with the instrument turned on press and hold simultaneously the two QNH push buttons. After a few seconds the displayed units system will change and be memorized.

One of these unit systems can be changed from:

- Metric:**
 - Altitude in meters with QNH in mbar
 - ASI in km/h
 - VSI in m/s

- Temperatures in Celsius
- Oil pressure in bar
- Fuel consumption in liters/h

2. Imperial (US)


- Altitude in ft with QNH in inches of Hg
- ASI in mi/h (Statute)
- VSI in ft/min
- Temperatures in Fahrenheit
- Oil pressure in PSI
- Fuel consumption in gal (US)/h

3. Imperial (UK)

- Altitude in ft with QNH in mbar
- ASI in mi/h (statute) or knots
- VSI in ft/min
- Temperatures in Celsius
- Oil pressure in bar
- Fuel consumption in liters/hour

The sequence of change is:

Metric
Imperial (UK)
Imperial (US)
Imperial (UK) ASI in knots



NOTE

For Australian operations CASA requires the units to be set to Imperial (UK) with ASI in knots.

7.17 Emergency Parachute – Optional Equipment

NOTE

The parachute is optional unless governing body of the country where the aircraft is to be flown requires a parachute.

WARNING

THE BRS EMERGENCY PARACHUTE RECOVERY SYSTEM INSTALLATION HAS BEEN APPROVED BY CASA ON THE BASIS THAT, WHILST NOT DEPLOYED, IT WILL NOT HAZARD THE AEROPLANE, ITS OCCUPANTS OR GROUND PERSONNEL. CASA HAS NOT APPROVED THE SYSTEM ITSELF OR CONSIDERED IN WHAT CIRCUMSTANCES, IF ANY, IT MIGHT BE USEFULLY DEPLOYED, THE SYSTEM HAS NOT BEEN DEMONSTRATED TO BE EFFECTIVE IN SAFELY RECOVERING THE AEROPLANE.

The BRS emergency parachute system has a double acting firing mechanism. The parachute-operating handle is fitted with a safety pin and is located on the left side of the seat frame. This pin should be removed before each flight and the safety pin must be replaced before the pilot alights from the aircraft. A force of approximately 15 – 20 kg pull on the actuating handle is required to activate the BRS rocket motor

Emergency procedures for use of the BRS can be found in section 3.3.9 of this manual. Additional information including service and maintenance requirements can be found in the BRS manual.

7.18 Ignition Switches

The ignition switches are located on the lower right side of the seat frame. Operation is forward for on and rearward for off. When the switches are in the off position the switch shorts the engine coils to earth causing the engine to stop.

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HANDLING SERVICE AND MAINTENANCE

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8 HANDLING SERVICE AND MAINTENANCE

8.1 Introduction

This section contains factory recommended procedures for proper ground handling and routine care for your Edge XT 912 Streak aircraft. Included in this section is relevant information required by the operator.

WARNING

IT IS THE PILOTS RESPONSIBILITY TO ENSURE THAT ALL AIRWORTHINESS DIRECTIVES HAVE BEEN ADDRESSED. IT IS ALSO THE PILOTS RESPONSIBILITY TO ENSURE SERVICING AND MAINTENANCE HAS BEEN PERFORMED AS OUTLINED IN THE APPROPRIATE MAINTENANCE MANUAL AND IN ACCORDANCE WITH THE APPLICABLE AVIATION REGULATIONS.

8.2 Identification Plate

The aircraft has two identification plates. The wing identification plate can be found on the negative plate of the universal bracket. The base identification plate can be found on the seat mast block on the left side of the aircraft. The Serial number should be quoted when corresponding with the factory.

8.3 Aircraft Documents

The Pilot's Operating Handbook is one of a series of documents required to safely operate this aircraft. A document list can be found in section 0 of this manual under DATA PACKAGE.

8.4 Aircraft Inspection, Maintenance and repair

Maintainer qualifications vary from country to country. The operator / maintainer should be familiar with the local requirements. Maintenance requirements are outlined in the base maintenance manual for the base unit and in the wing maintenance manual for the wing. The following sections have been included because it is considered that the information may be required on a more regular basis.

8.5 Fuel System

8.5.1 Filling Fuel Tanks

The properties of the fuel tank material cause an increase in capacity after the first 2 to 3 tanks of fuel. Initial capacity is 64 litres with the "aged" capacity 70 litres. The fuel level markings have been positioned for the fuel tank capacity at 70 litres.

The XT has a single fuel tank. When the tank is being filled there may be a slight pressure differential between the sides of the tank, causing the fuel cap side to fill slightly faster than the other side. Allow time for the breather valves to equalise the pressure to allow complete filling and, check that both sides are sufficiently full. Fill to the neck of the fuel entrance.

8.5.2 Fuel Specification

FUEL

| | |
|-----------|--|
| Fuel type | En228 Premium/Regular. Super grade gasoline, lead free, min RON 90 |
|-----------|--|

Table 1 Section 8. Fuel Specification

Refer to the Rotax manual section 10-10 for engine fuel specifications that apply to the region where the aircraft is being flown.

Use of AvGas requires higher frequency maintenance intervals. If AvGas is used the Rotax web site should be referenced for maintenance requirements. See Rotax service information 18-UL-97-D/E Refer to section 2.12 for fuel capacities and limitations

8.5.3 Fuel Sampling

There is a draincock on the base of the fuel tank at the left hand side, which may be used to check the quality of the fuel, and to drain fuel if necessary, it is especially important to remove any water that may have been introduced from the system.

8.5.4 Checking Fuel

The fuel is checked for water and contaminants by draining a sample of the fuel into a clear glass container. Once a sample has been taken the quality of the fuel can be checked by looking for any water at the bottom of the glass, and checking for any other visual contaminants.

If the fuel has been sitting for an extended period without use it may be advisable to replace it with fresh fuel.

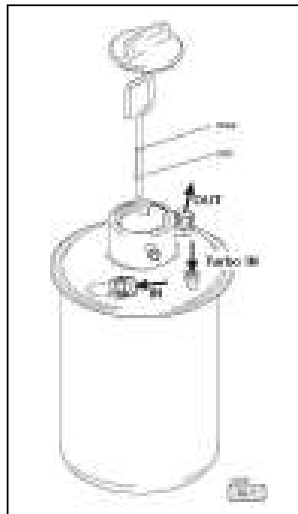
8.5.5 Draining the Fuel

Ensure that a suitable receptacle is found for the fuel that is to be drained, position the trike above the receptacle and depress the draincock. Ensure that there are no ignition sources and that the fuel is disposed of correctly.

8.6 Engine Oil System Replenishment

The minimum oil level is 2 litres, max 3 litres. This checked and replenished by removing the oil sump lid. Rotax has provided service instructions, which detail how to check the oil.

Removing the sump plug drains the sump. Ensure that the sump plug is correctly replaced and lock wired prior to refilling the engine with oil. Measure the amount to be replaced, refill, check the level, run the engine and recheck. The opportunity should be taken to replace the oil filter any time that the oil is replaced.



Oil Level Instructions:

Do Not overfill the oil system. The difference between the min and max marks on the dipstick is 0.45 litres.

Figure 1 Section 8. Oil Dipstick Diagram

Rotax Service instructions should be consulted (SI-27-1997 R1)

8.6.1 Lubricating Oil

The 912 UL engine has an external sump, and the entire system is standard to the Rotax 912 engine. The oil specification is given in the Rotax Operators Manual, Section 10.2.3, Lubricants. In general use only synthetic or semi synthetic oil, API classification "SF" or "SG" or later oils. Multigrade is

recommended. These oil types are detergent types. Consult the Rotax manual and Rotax service instruction 18 UL 97, for the correct type and grade of oil for the ambient operating temperature.

Two oils, which are recommended by the Rotax Service instruction 18, UL 97 for use with both Avgas and Unleaded fuels are:

SHELL, Advance VSX 4, APISG, SAE 15W-50

VALVOLINE, Dura Blend Synthetic, APISJ, SAE 10W-40

8.7 Cooling System

WARNING

DO NOT OPEN THE COOLING SYSTEM WHEN THE ENGINE IS HOT. SEVERE SCALDING AND OTHER INJURIES MAY RESULT.

Water-cooling system capacity is 2.5 l. See maintenance manual for further details.

Coolant Specification

A MANDATORY Rotax Directive was issued on the 25th of November 2004 which changes the type of coolant that must be used with the rotax 912 type engine. The new coolant is Evans NPG+ waterless coolant. The reason for the change is "In some instances conventional coolant (mixture ratio of 50% water and 50% antifreeze) can vaporize or boil before the maximum permissible cylinder head temperature is reached." Rotax Service bulletin SB-912-043, pg # 1.

Some Airborne trikes will have a silicate free type high quality and long life antifreeze coolant (which is red), Airborne Part Number 106644, installed in the radiator. This coolant must be changed to the newly recommended coolant by the 25th of Feb 2005, or after 100hrs operating time of the notice.

The directive requires that the new coolant be used, and a sticker be placed on the coolant cap which prohibits the use of water in the coolant system.

The coolant should be replaced according to the rotax maintenance manual, current issue. Please refer to the directive, which is available from the rotax website: SB-912-043, September 04.

WARNING

WATER OR WATER CONTAINING COOLANT MUST NOT BE ADDED IN ANY CASE TO THE COOLING SYSTEM WITH THE NEW EVANS NPG+ COOLANT.

Field service Instructions:

"If EVANS NPG+ coolant is not locally available, temporarily top off the system with propylene glycol antifreeze and be sure not to add water. Within 15 days the temporary coolant should be completely drained and the system refilled with EVANS NPG+ coolant." Rotax SB-912-043, Pg # 5.

8.8 Tyre Inflation

The recommended tyre inflation pressures are 15 Psi for both the front and rear tyres. When checking the tyre pressures the opportunity should be taken to examine the tyres for wear, cuts, bruises, slippage and other defects.

8.9 Shock Struts

8.9.1 Rear Shocks

WARNING

SPECIALISED PUMPS MUST BE USED FOR THE AIR SHOCKS – PRESSURES UP TO 600 PSI EXIST.

The rear shocks are pressurised to 580 psi using a schrader valve system. A special pump will be necessary to repressurise the rear shocks to the correct setting. There should be no reason why the rear shock would need to be reinflated, and if they do then a proper investigation of the cause should be undertaken.

8.9.2 Front Shocks

The front shocks should be inflated to 50 psi for each reservoir. The top should be inflated first. Turn the rebound damper fully clockwise, and then anticlockwise for twelve clicks.

8.9.3 Circuit Breaker and Fuses

The fuses for the electrical equipment are located in two positions.

1. The Lynx intercom system has a 1.5A fuse screwed into the side of the box where the headsets and push to talk cables are plugged in.
2. The power supply cables for the radio are protected at the rear of the aircraft with inline fuses which terminate at the right hand side of the mast block. A 5A fuse is to be used for the radio and intercom power supply. The fuse holder is marked with the correct Current rating for the fuse.
3. The battery charging circuit is protected with a 20A fuse, which also terminates at the right hand side of the mast block. The fuse holder is marked with the correct Current rating for the fuse.
4. A 10 amp circuit breaker is located on the right hand side of the dash. The circuit breaker protects the dash instrumentation and the DC socket.

8.10 Parking and ground handling

Parking and ground handling information can be located in section 4.

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SUPPLEMENTS

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9 SUPPLEMENTS

9.1 *Introduction*

The supplement section of this manual is intended to provide details and procedures associated with the fitment of optional and special purpose equipment.

At this time there are no supplements required for the aircraft.